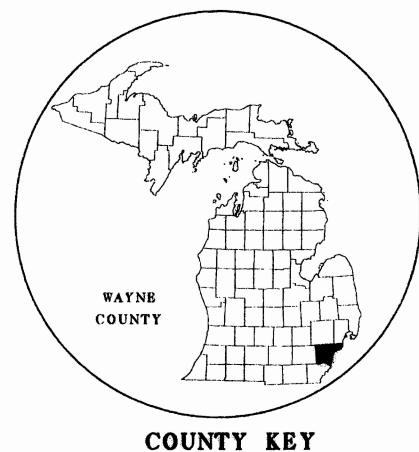


1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16 17 18 19 20 21 22 23 24 25 26 27 28 29 30 31 32 33 34 35 36 37 38 39 40 41 42 43 44 45 46 47 48 49 50 51 52 53 54 55 56 57 58 59 60 61 62 63

ROAD
PLANS

SHEET
NUMBERS

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TITLE SHEET LEGEND

PROPOSED PROJECT	
EXISTING ROADS	
PAVED	
BITUMINOUS	
GRAVEL	
UNIMPROVED OR CITY STREET	
SECTION LINE	
TOWNSHIP LINE	
COUNTY LINE	
CITY OR VILLAGE LIMITS	
RAILROADS	

MICHIGAN
DEPARTMENT OF TRANSPORTATION

PLANS OF PROPOSED

MICHIGAN PROJECT STRG 0682(356)

CONTROL SECTION 82211

JOB NUMBER 73160A

M-85 AT G01 OF 82211, CN/GTW RAILROAD CROSSING

SECTION 24, T4S, R10E
CITY OF TRENTON
WAYNE COUNTY



G01 OF 82211
M-85 @ CN/GTW RR
RAILROAD CROSSING
CS 82211 - JN 73160A,
JN 73164A
P.O.B. STA 467+15.00
P.O.E. STA 470+03.50
C.S. BMP = 3.984
C.S. EMP = 4.040
P.R. *1592105 (NB M-85)
P.R. BMP=4.076
P.R. EMP=4.106
P.R. *1592106 (SB M-85)
P.R. BMP = 3.916
P.R. EMP=3.948

JN 73164A
CN/GTW RR
FORCE ACCOUNT

THE IMPROVEMENTS COVERED BY THESE PLANS SHALL BE DONE IN ACCORDANCE WITH THE MICHIGAN DEPARTMENT OF TRANSPORTATION 2003 STANDARD SPECIFICATIONS FOR CONSTRUCTION.

	2006	2026
A.D.T. NB	18,100	22,100
A.D.T. SB	18,100	22,100
D.H.V. NB	1,810	2,210
D.H.V. SB	1,810	2,210
COMM. Z	3.32	4.52
DESIGN SPEED	55 MPH	
POSTED SPEED	50 MPH	

LENGTH - 0.055 MILES

CONTRACT FOR REPLACEMENT OF RAILROAD CROSSINGS ON M-85 (FORT STREET) NB AND SB, AND ADJACENT BIKEPATH TRAIL, ASSOCIATED ROADWORK, AND MAINTAINING TRAFFIC.

THESE PLANS WERE PREPARED FOR THE
MICHIGAN DEPARTMENT OF TRANSPORTATION
BY
SPALDING DeDECKER ASSOCIATES, INC.



ENGINEERING & SURVEYING EXCELLENCE
SINCE 1954
400 SOUTH BLVD EAST
HOCKESSER HILLS, MI 48037
(248) 844-5400
(248) 844-5404 Fax



PROJECT MANAGER SPALDING DeDECKER ASSOC.

APPROVALS

RECOMMENDED FOR APPROVAL		3/20/06
	GOKTGE YUNG, P.E. - PROJECT MANAGER	DATE
RECOMMENDED FOR APPROVAL		03-20-06
	JOHN F. SANFORD, P.E. - LICENSED ENGINEER	DATE

MICHIGAN
DEPARTMENT OF TRANSPORTATION
KIRK T. STEUDLE, P.E. DIRECTOR

APPROVED BY 3/27/06
ENGINEER OF DEVELOPMENT DATE



JEFFREY A. HORNE
MDOT DESIGN COORDINATOR
G. YUNG
DESIGN UNIT

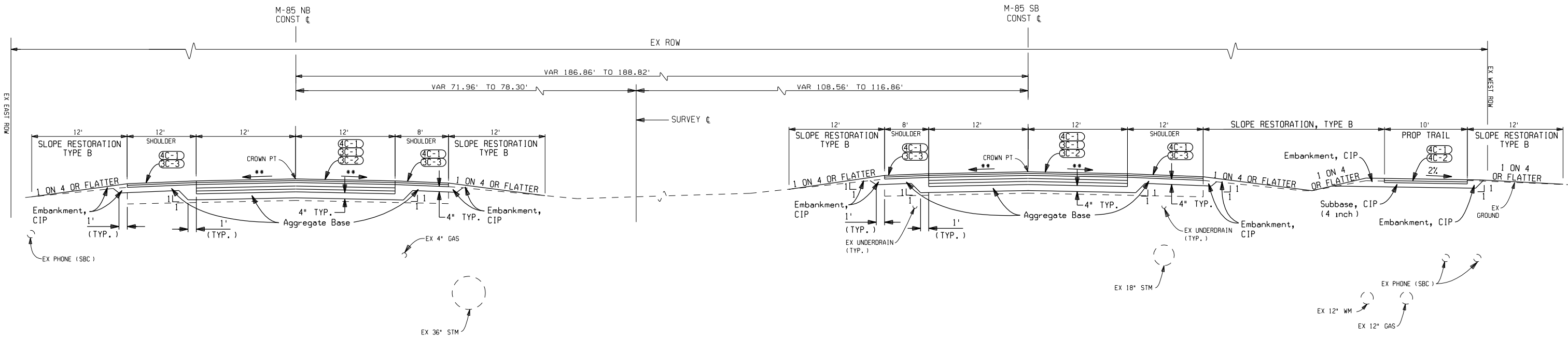
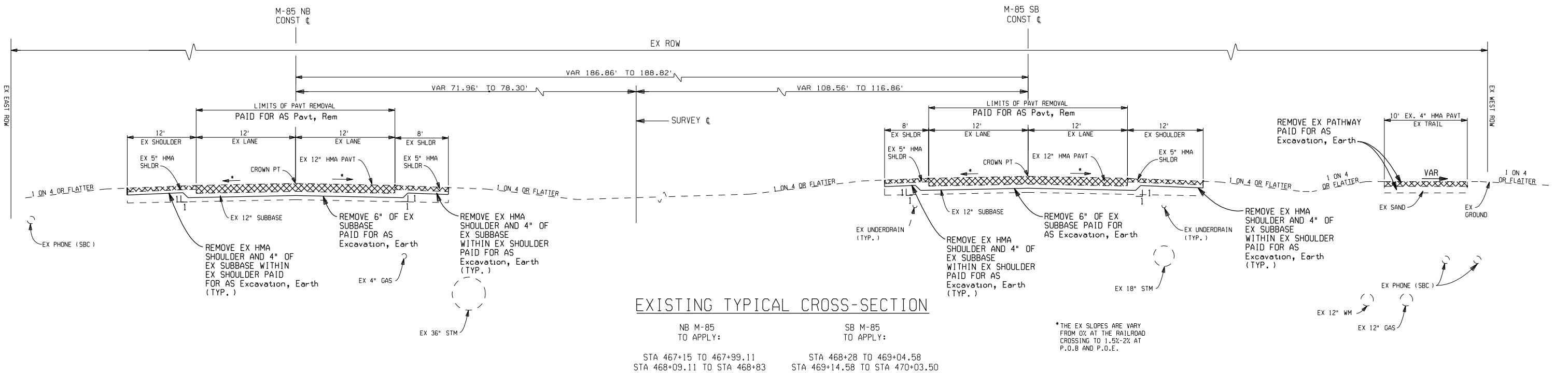
CONTROL SECTION	JOB NUMBER	FEDERAL NUMBERS PROJECT	ITEM	SHEET NO.
82211	73160A	STRG 0682(356)	JJ2431	1

CS 82211 - JN 73160A

JOB NUMBER
CONTROL SECTION

CHECKED BY: JAS DATE: 3/27/06
APPROVED BY: ME DATE: 3/27/06

FINAL R.O.W.			
AUTH	DATE	NO.	REVISION



HMA APPLICATION ESTIMATE

IDENT NO.	ITEM	RATE PER SYD	PERFORMANCE GRADE	REMARKS
4C-1	HMA, 4C	220	64-22	TOP COURSE AWI=260
4C-2	HMA, 4C	220	64-22	BASE COURSE
3C-1	HMA, 3C	220	64-22	BASE COURSE
3C-2	HMA, 3C	1100	64-22	BASE COURSE IN 3 LIFTS
3C-3	HMA, 3C	330	64-22	BASE COURSE
	* BOND COAT	0-0.10 GAL		

*FOR INFORMATION ONLY

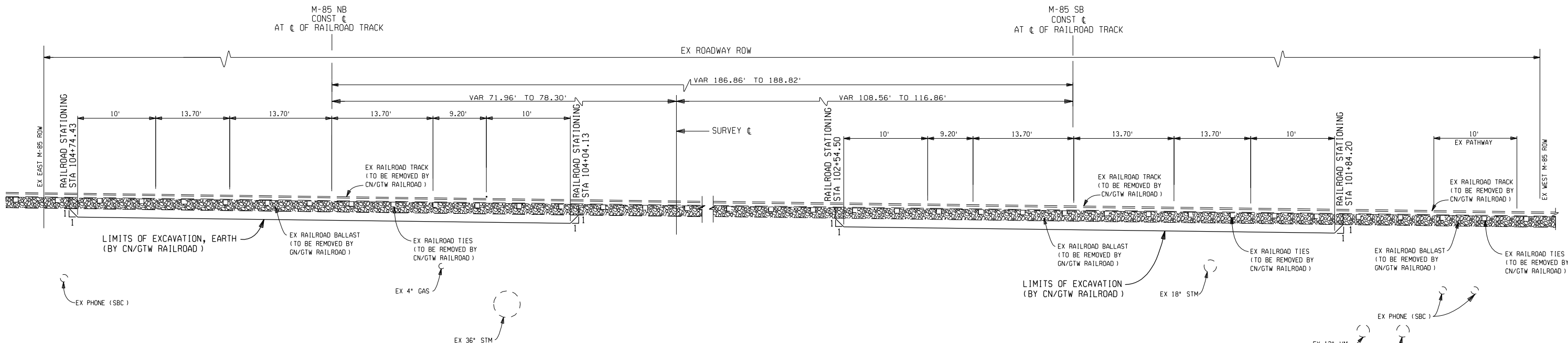
TYPICAL CROSS-SECTIONS



M-85 AT GO1 OF 82211, CN/GTW RAILROAD CROSSING

DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO.
03/10/06	1" = 6'	82211	73160A	YUNG	2

FINAL R.O.W.			
AUTH	DATE	NO.	REVISION

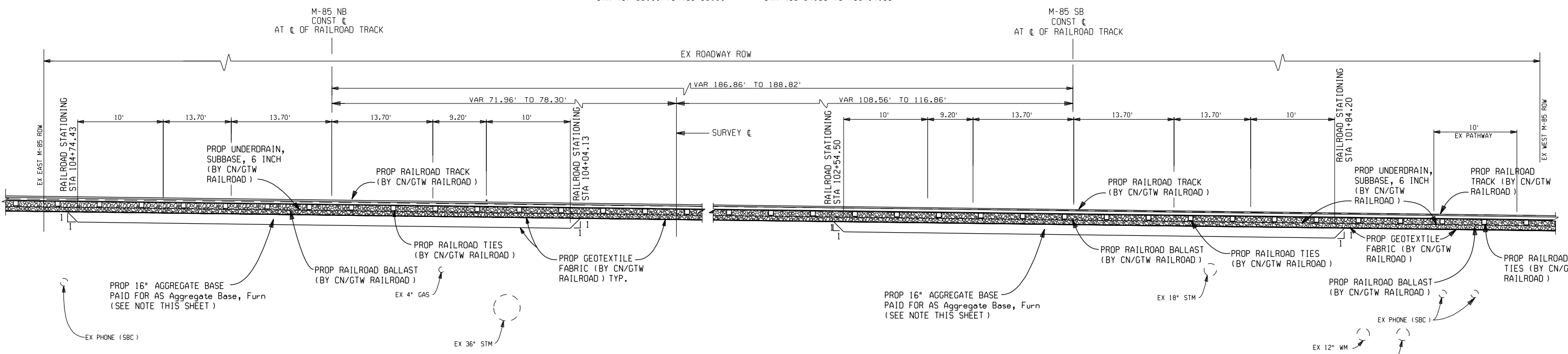


EXISTING TYPICAL CROSS-SECTION
THROUGH RAILROAD CROSSING

NB M-85
TO APPLY:
STA 467+99.11 TO 468+09.11

SB M-85
TO APPLY:
STA 469+04.58 TO 469+14.58

NOTE:
ALL DIMENSIONS ARE SHOWN ALONG
CEINTER LINE OF RAILROAD TRACKS




PROPOSED TYPICAL CROSS-SECTION
THROUGH RAILROAD CROSSING

NB M-85
TO APPLY:
STA 467+99.11 TO 468+09.11


SB M-85
TO APPLY:
STA 469+04.58 TO 469+14.58

NOTE:
THE CONTRACTOR SHALL FURNISH THE AGGREGATE BASE MATERIAL
AND STORE ON THE CONSTRUCTION SITE AT THE LOCATION DIRECTED
BY THE ENGINEER INCLUDED IN Aggregate Base, Furn.
THE CN/GTW RR WILL PLACE AND COMPACT AGGREGATE BASE MATERIAL

TYPICAL CROSS-SECTIONS



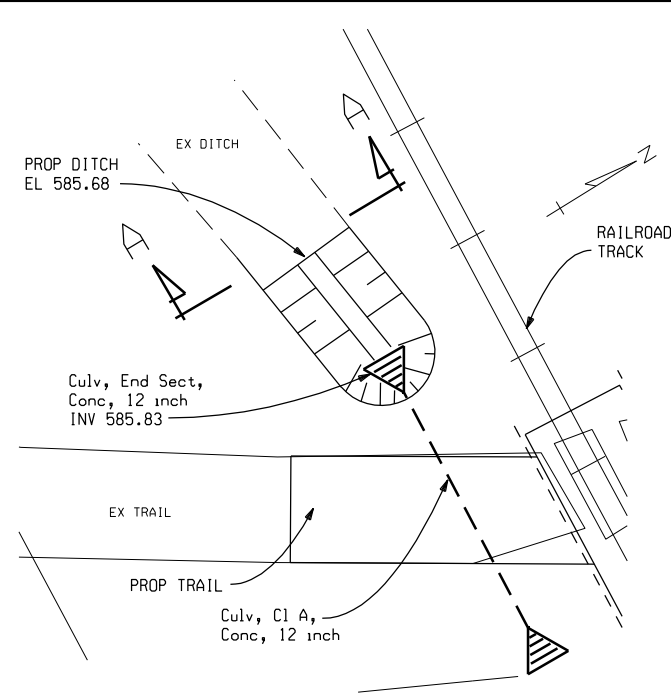
150 9001 CERTIFIED
ENGINEERING & SURVEYING EXCELLENCE
SINCE 1964



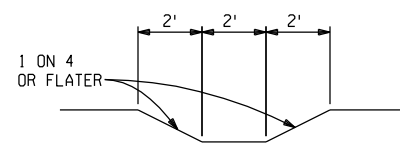
Michigan Department of Transportation

M-85 AT GO1 OF 82211, CN/GTW RAILROAD CROSSING					
DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO.
03/10/06	1" = 6'	82211	73160A	YUNG	R.O.W CONST. 3

FINAL R.O.W.			
AUTH	DATE	NO.	REVISION

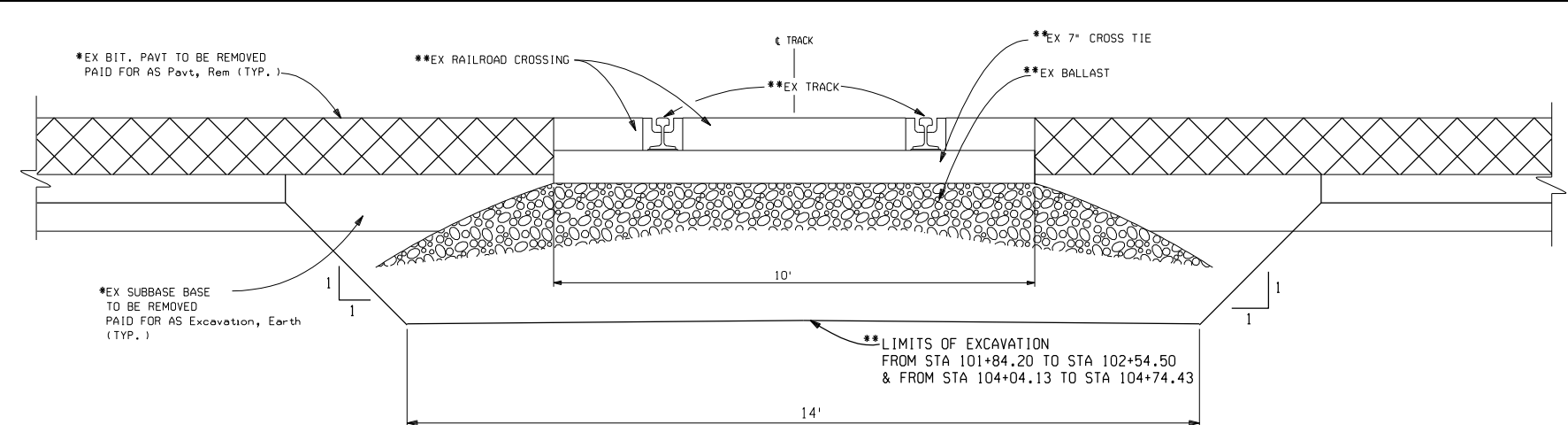


DETAIL A
AT TRAIL/RAILROAD CROSSING
FOR LOCATION SEE SHEET 9



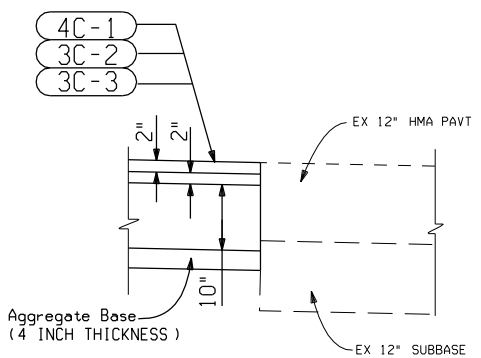
GRADE TO HERE
INCLUDED IN
Excavation, Earth
(TO BE DONE BY
THE CONTRACTOR)

SECTION A-A

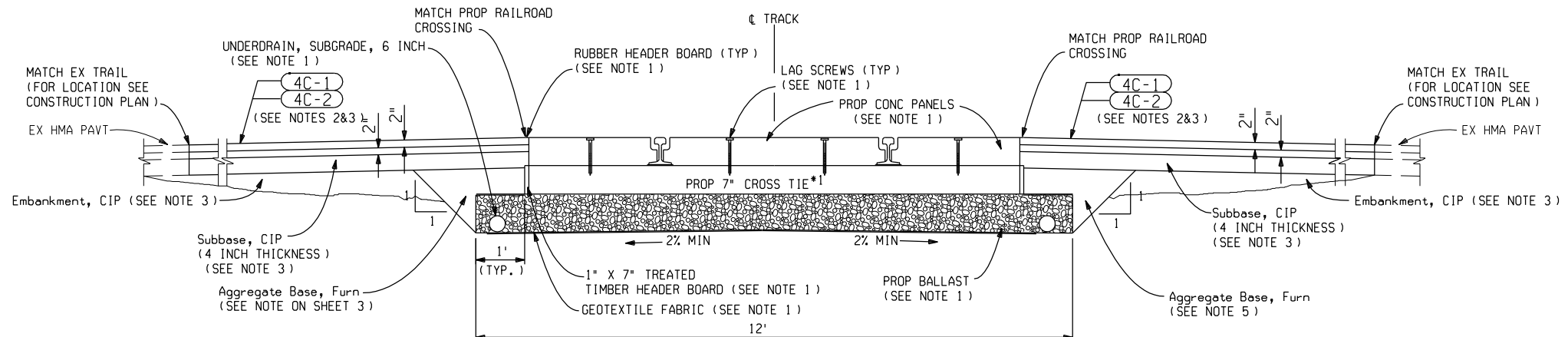


EXISTING M-85 & CN/GTW RAILROAD CROSSING DETAIL

APPLIES:
STA 101+50 TO STA 101+71.50
STA 101+84.20 TO STA 102+54.50 (RAILROAD STATIONING)
STA 104+04.13 TO STA 104+74.43 (RAILROAD STATIONING)

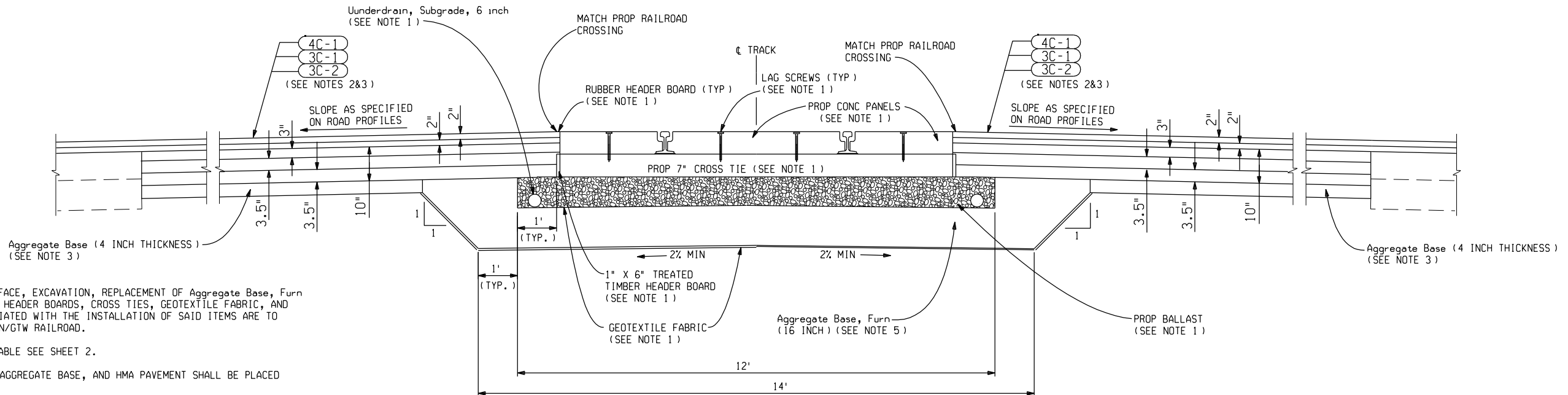


BUTT JOINT DETAIL



PROPOSED TRAIL & CN/GTW RAILROAD CROSSING DETAIL

APPLIES:
STA 101+50 TO STA 101+71.50 (RAILROAD STATIONING)



PROPOSED M-85 & CN/GTW RAILROAD CROSSING DETAIL

APPLIES:
STA 101+84.20 TO STA 102+54.50 (RAILROAD STATIONING)
STA 104+04.13 TO STA 104+74.43 (RAILROAD STATIONING)

- NOTE:
- RAILROAD CROSSING SURFACE, EXCAVATION, REPLACEMENT OF Aggregate Base, Furn BALLAST, UNDERDRAINS, HEADER BOARDS, CROSS TIES, GEOTEXTILE FABRIC, AND ALL OTHER LABOR ASSOCIATED WITH THE INSTALLATION OF SAID ITEMS ARE TO BE PERFORMED BY THE CN/GTW RAILROAD.
 - FOR HMA APPLICATION TABLE SEE SHEET 2.
 - EMBANKMENT, SUBBASE, AGGREGATE BASE, AND HMA PAVEMENT SHALL BE PLACED BY THE CONTRACTOR.
 - RAILROAD SHALL REMOVE HMA PAVEMENT LOCATED WITHIN THE TIE LIMITS AND STORE IT ON THE SITE. THE CONTRACTOR SHALL REMOVE THIS HMA MIXTURE FROM THE SITE, INCLUDED IN "Excavation Earth".
 - THE CONTRACTOR SHALL FURNISH THE AGGREGATE BASE MATERIAL AND STORE ON THE CONSTRUCTION SITE AT THE LOCATION DIRECTED BY THE ENGINEER INCLUDED IN Aggregate Base, Furn. THE CN/GTW RR WILL PLACE AND COMPACT AGGREGATE BASE MATERIAL

MISCELLANEOUS DETAILS



M-85 AT GO1 OF 82211, CN/GTW RAILROAD CROSSING					
DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO.
03/10/06	NONE	82211	73160A	YUNG	R.O.W CONST. 4

METHOD OF SURVEY REGULAR - PARTIAL		YEAR 2005
SURVEY ORDER	SURVEY CHIEF JEFFREY BARLETT, P.S.	YEAR 2005
AERIAL SURVEY NO	N/A	YEAR
HORIZ DATUM	NAD83	VERT DATUM NAVD88
ROAD DESIGN INITIATED JANUARY 2006		COMPLETED FEB 2006
PRELIMINARY PLANS BY	SDA	FINAL PLANS BY SDA
FIELD INSPECTION (GI) BY N/A		DATE
FHWA AREA ENGINEER		

GENERAL PLAN NOTES

UNDERGROUND UTILITIES

FOR PROTECTION OF UNDERGROUND UTILITIES AND IN CONFORMANCE WITH PUBLIC ACT 53, 1974, THE CONTRACTOR SHALL DIAL 1-800-482-7171 A MINIMUM OF THREE FULL WORKING DAYS,EXCLUDING SATURDAYS, SUNDAYS, AND HOLIDAYS PRIOR TO BEGINNING EACH EXCAVATION IN AREAS WHERE PUBLIC UTILITIES HAVE NOT BEEN PREVIOUSLY LOCATED. MEMBERS WILL THUS BE ROUTINELY NOTIFIED. THIS DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF NOTIFYING UTILITY OWNERS WHO MAY NOT BE A PART OF THE "MISS DIG" ALERT SYSTEM.

MDOT'S FREEWAY LIGHTING SYSTEM, INTELLIGENT TRANSPORTATION SYSTEMS (ITS) AND OTHER MISCELLANEOUS ELECTRICAL SYSTEMS ARE NOT A PART OF MISS DIG. THEREFORE, CONTRACTORS SHALL CALL:

FREEWAY LIGHTING- MDOT METRO REGION MAINTENANCE FREEWAY LIGHTING COORDINATOR:(248)83-5125

ITS- METRO FREEWAY ITS OPERATIONS CENTER: (313)256-9800 EXT 310, OR (313)965-0777

CONTRACTORS SHALL CONTACT THE MAINTENANCE REPRESENTATIVE AT THE MDOT REGION/TSC OFFICE TO HAVE MDOT ELECTRICAL SYSTEMS STAKED.

PLAN ELEVATION

ELEVATIONS SHOWN ON THESE PLANS ARE FROM TOPOGRAPHICAL SURVEY TAKEN IN MAY 2005.

OLD PLANS

OLD PLANS THAT PREDATE THIS PROJECT MAY BE AVAILABLE. THESE PLANS MAY BE REVIEWED IN THE TRANSPORTATION SERVICE CENTER DURING NORMAL WORKING HOURS. CS 82211 JN 27448

STATIONING

SURVEY t ON THIS PROJECT WAS TAKEN FROM OLD PLANS AND IS NOT NECESSARILY ACCURATE.

VEHICLES AND MATERIALS STORAGE

THE CONTRACTOR SHALL NOT PARK ANY VEHICLES NOR STORE ANY MATERIALS ON PUBLIC RECREATIONAL PROPERTY.

PAVEMENT REMOVAL QUANTITIES

PAVEMENT REMOVAL AS SHOWN ON THE PLANS SHALL BE AT THE DISCRETION OF THE ENGINEER. IF IN HIS/HER JUDGEMENT, AREAS OF PAVEMENT MAY BE LEFT IN PLACE, OR ADDITIONAL AREAS ADDED TO PROVIDE THE PROPER CROSS-SECTION AND BASE, CHANGES CAN BE MADE IN THE QUANTITIES.

HORIZONTAL/VERTICAL DATUM

THE HORIZONTAL CONTROL IS BASED ON MICHIGAN STATE PLANE COORDINATE SYSTEM. THE VERTICAL CONTROL DATUM IS NAVD 88.

STATE PLANE COORDINATES AND DISTANCES

ALL DISTANCES AND COORDINATES ON THIS PROJECT ARE ON THE STATE PLANE COORDINATE SYSTEM. THE CONSTANT AVERAGE SCALE FACTOR IS 0.999970853

STATE PLANE DISTANCE / 0.999970853 = GROUND DISTANCE.

SAW CUTTING FOR PAVEMENT REMOVAL

WHERE EXISTING PAVEMENT IS TO BE REMOVED ADJACENT TO PAVEMENT REMAINING IN PLACE THE PAVEMENT SHALL BE SAW CUT FULL DEPTH FOR THE ENTIRE REMOVAL LENGTH AND WIDTH. THIS WORK SHALL NOT BE PAID SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR REMOVING PAVEMENT. ALL OTHER SAW CUTTING THAT MAY BE NECESSARY WILL BE CONSIDERED INCLUDED IN THE WORK WHICH IS BEING PERFORMED AND WILL NOT BE PAID FOR SEPARATELY.

SLOPES

CLASS A SLOPES SHALL BE REQUIRED ON THIS PROJECT.

SOIL EROSION MEASURES

APPROPRIATE SOIL EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE IN PLACE PRIOR TO EARTH-DISTURBING ACTIVITIES. PLACE TURF ESTABLISHMENT ITEMS AS SOON AS POSSIBLE ON POTENTIAL ERODABLE SLOPES AS DIRECTED BY THE ENGINEER. CRITICAL DITCH GRADES SHALL BE PROTECTED WITH EITHER SOD OR SEED/MULCH OR MULCH BLANKET AS DIRECTED BY THE ENGINEER.

SOIL BORINGS AND/OR PAVEMENT CORES

THE SOIL BORING LOGS AND/OR PAVEMENT CORES REPRESENT POINT INFORMATION. NO INFERENCE SHOULD BE MADE THAT SUBSURFACE OR PAVEMENT CONDITIONS ARE THE SAME AT OTHER LOCATIONS.

EARTHWORK

EARTHWORK QUANTITIES ARE COMPUTED BY CONVENTIONAL SURVEY METHODS.

PAVEMENT MARKINGS AND SIGNS

ALL PERMANENT PAVEMENT MARKINGS, SHAPES, AND DIMENSIONS SHALL CONFORM WITH MDOT PAVEMENT MARKING TYPICALS VIII-900 THRU 990 SERIES UNLESS SPECIFIED BY SPECIAL PROVISION. ALL EDGE LINES SHALL BE 6 INCHES AND ALL GORE MARKINGS SHALL BE 12 INCHES. OFFSETS FROM CONSTRUCTION JOINTS SHALL REMAIN AS NOTED IN THE PAVEMENT MARKING TYPICALS. ALL PERMANENT SIGNS SHALL CONFORM WITH MDOT SIGNING TYPICAL PLANS VIII-100 THRU 830 SERIES UNLESS SPECIFIED BY SPECIAL PROVISION.

PERMANENT SIGNS

ANY PERMANENT SIGNS REQUIRING RELOCATION DUE TO CONTRACTOR OPERATIONS SHALL BE SALVAGED AND RESET BY THE CONTRACTOR AT LOCATIONS DESIGNATED BY THE ENGINEER. THE COST OF THIS WORK SHALL BE BORNE BY THE CONTRACTOR.

AGGREGATE BASE UNDER HMA

AGGREGATE BASES TO BE SURFACED WITH HMA SHALL USE AGGREGATE 22A, UNLESS OTHERWISE SPECIFIED.

UTILITIES

THE EXISTING UTILITIES LISTED BELOW AND SHOWN ON THESE PLANS REPRESENT THE BEST INFORMATION AVAILABLE AS OBTAINED ON OUR SURVEYS DATED MARCH 2004. THIS INFOR- MATION DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY TO BE SATISFIED AS TO ITS ACCURACY AND THE LOCATION OF EXISTING UTILITIES.

NAME OF OWNER	KIND OF UTILITY
SBC 31100 PLYMOUTH ROAD ROOM 301 LIVONIA, MI 48150 GREG GIANNETTI (734) 523-7572	TELEPHONE
DTE ENERGY 2000 2ND AVE. ROOM 518 DETROIT, MI 48226 ANJANETTE BORAWSKI (313) 235-9284	ELECTRIC
MICHCON 3200 HOBSON DETROIT, MI 48201 (313) 577-7327	GAS
DEPARTMENT OF PUBLIC WORKS 1431 WEST ROAD TRENTON, MI 48183 LAWRENCE DUSINCKI (734) 675-8470	WATER & SEWER
QWEST COMMUNICATIONS 1860 LINCOLN STREET, 2ND FLOOR DENVER, COLORADO 80295 GEORGE W. McELVAIN (303) 837-3984	FIBER OPTIC CABLE

THE LOCATION OF ALL PUBLIC UTILITIES SHOWN ON THESE PLANS IS TAKEN FROM THE BEST AVAILABLE DATA. THE MICHIGAN DEPARTMENT OF TRANSPORTATION WILL NOT BE RESPONSIBLE FOR ANY OMISSION OR VARIATIONS FROM THE LOCATIONS SHOWN. PURSUANT TO ACT 53 OF THE PA OF 1974 AS A CONDITION OF THIS CONTRACT NOTICE SHALL BE GIVEN TO MISS DIG 3 WORKING DAYS PRIOR TO UNDERGROUND WORK TO BE PERFORMED IN ACCORDANCE WITH THIS CONTRACT. PHONE (800) 482-7171 OR 647-7344

NOTES APPLYING TO STANDARD PLANS

WHERE THE FOLLOWING ITEMS ARE CALLED FOR ON PLANS, THEY ARE TO BE CONSTRUCTED ACCORDING TO THE STANDARD PLAN GIVEN BELOW OPPOSITE EACH ITEM UNLESS OTHERWISE INDICATED.

GRANULAR BLANKET, UNDERDRAINS, OUTLET ENDINGS FOR UNDERDRAINS, AND SEWER BULKHEADS	R-80-C
BEDDING AND FILLING AROUND PIPE CULVERTS	R-82-D
PRECAST CONCRETE END SECTION FOR PIPE CULVERT	R-86-D
SOIL EROSION AND SEDIMENTATION CONTROL MEASURES	R-96-C
SODDING, SEEDING AND TREE PLANTING	R-100-D
TRACK CROSSINGS	R-121-B
TEMPORARY TRAFFIC CONTROL DEVICES	R-125-D*

* SPECIAL DETAIL

MISCELLANEOUS ITEMS - JN 73160A

1	Ls	Contractor Staking*
2	Hr	Staking Plan Errors and Extras, One Person
1	Hr	Staking Plan Errors and Extras, Two Person
1	Hr	Staking Plan Errors and Extras, Three Person
80	Cyd	Subgrade Undercutting, Type II
2,883	Syd	Slope Restoration, Type B
28	Unit	Water, Sodding/Seeding
10	Ea	Dr Marker Post
50	Ft	Exploratory Trenching
20	Cyd	Non Haz Contaminated Material Handling and Disposal, LM
262	Ton	Aggregate Base, Furn
8	Ea	Underdrain, Outlet Ending, Furn
10	Cyd	Maintenance Gravel, LM

* THE "Contractor Staking" SHALL INCLUDE STAKING TOP OF THE RAIL ELEVATIONS AND LIMITS OF EXCAVATION UNDER THE RAILROAD CROSSING.

SOIL EROSION AND SEDIMENTATION CONTROL ITEMS:

1,100	Ft	Erosion Control, Silt Fence
3	Ea	Erosion Control, Sediment Trap
20	Cyd	Erosion Control, Maintenance, Sediment Removal
9	Syd	Riprap, Plain

MAINTAINING TRAFFIC ITEMS

30	Ft	Fence, Protective
1,000	Ft	Post, Wood, 6 inch by 8 inch, Direct Embedment
3	Ea	Barricade, Type III, High Intensity, Lighted, Furn
3	Ea	Barricade, Type III, High Intensity, Lighted, Oper
1	Ea	Barricade, Type III, High Intensity, Double Sided, Lighted, Furn
1	Ea	Barricade, Type III, High Intensity, Double Sided, Lighted, Oper
2	Ea	Lighted Arrow, Type C, Furn
2	Ea	Lighted Arrow, Type C, Oper
1	Ls	Minor Traf Devices
600	Ft	Pavt Mrkg, Longit, 6 inch or Less Width, Rem
1850	Ft	Pavt Mrkg, Type R, 4 inch, White, Temp
1700	Ft	Pavt Mrkg, Type R, 4 inch, Yellow, Temp
200	Ea	Plastic Drum, High Intensity, Lighted, Furn
200	Ea	Plastic Drum, High Intensity, Lighted, Oper
6	Ea	Sign Cover
1	Ea	Sign, Portable, Changeable Message, Furn
1	Ea	Sign, Portable, Changeable Message, Oper
670	Sft	Sign, Type B, Temp, Furn
670	Sft	Sign, Type B, Temp, Oper
2,400	Sft	Sign, Type B, Temp, Prismatic, Furn
2,400	Sft	Sign, Type B, Temp, Prismatic, Oper

PERMANENT PAVEMENT MARKINGS & DELINEATORS

700	Ft	Pavt Mrkg, Spray Thermopl, 4 inch, White
360	Ft	Pavt Mrkg, Spray Thermopl, 6 inch, White
360	Ft	Pavt Mrkg, Spray Thermopl, 6 inch, Yellow
50	Ft	Pavt Mrkg, Primerless Patterned Tape, 24 in, Stop Bar
















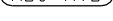


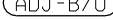











NOTE SHEET















DATE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO.	
03/10/06	82211	73160A	YUNG	R.O.W	CONST.
				5	

FINAL R.O.W.			
AUTH	DATE	NO.	REVISION














WATER & DRAINAGE SYMBOLS

-  EXISTING CATCH BASIN
-  PROPOSED CATCH BASIN
-  EXISTING MANHOLE
-  PROPOSED MANHOLE
-  EXISTING CULVERT END SECTION
-  PROPOSED CULVERT END SECTION
-  EXISTING HEADWALL
-  PROPOSED HEADWALL
-  WATER SHUTOFF (Service Valve)
-  GATE VALVE
-  GATEWELL
-  WATER METER
-  WATER MANHOLE
-  EXISTING FIRE HYDRANT
-  PROPOSED FIRE HYDRANT
-  ADJ-HYD ADJUST FIRE HYDRANT
-  ADJ ADJUST DRAINAGE STRUCTURE
-  ADJ-K ADJUST DRAINAGE STRUCTURE W/COVER
-  ADJ-B/O ADJUST DRAINAGE STRUCTURE BY OTHERS
-  REC RECONSTRUCT DRAINAGE STRUCTURE
-  REC-K RECONSTRUCT DRAINAGE STRUCTURE W/COVER
-  REL-B/O RELOCATE - BY OTHERS
-  SR-1 SIDEWALK RAMP TYPE
-  CHECK DAM (PROFILES)
-  DIKE (PROFILES)
-  W.T. WATER TABLE (PROFILES)
-  GUARD POST
-  WATER WELL

UTILITIES SYMBOLS

-  POWER POLE
-  TELEPHONE POLE
-  GUY POLE
-  LIGHT POLE
-  POWER LIGHT POLE
-  TELEPHONE MANHOLE
-  POWER TOWER
-  GAS VALVE
-  WALK/NO-WALK
-  DEADMAN FOR GUYWIRE
-  R X R RAILROAD SIGNAL
-  ELECTRICAL MANHOLE
-  ELECTRICAL HANDHOLE
-  TELEPHONE PEDESTAL/RISER

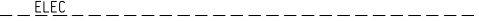


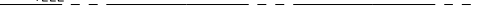




MISCELLANEOUS SYMBOLS

-  RIPRAP
-  SIGN
-  STUMP
-  SWAMP
-  DECIDUOUS TREE
-  EVERGREEN TREE
-  MAIL BOX
-  QUARTER CORNER
-  SECTION CORNER
-  HALF QUARTER SECTION
-  T.H.* TEST HOLE NO.
-  123 BEAM G. R. RUN NUMBER (EXISTING)
-  123 BEAM G. R. RUN NUMBER (PROPOSED)

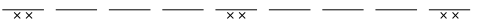




HAZARDOUS OR FLAMMABLE MATERIAL USED WITH UNDERGROUND GAS & ELECTRICAL LINES

CAUTION - CRITICAL UNDERGROUND UTILITY USED WITH FIBER OPTICS LINES







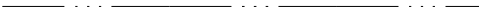

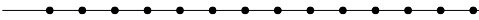
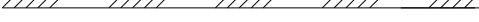




UTILITY PATTERNS

-  ELEC ELECTRICAL LINE
-  24" GAS GAS LINE
-  12" OIL OIL LINE
-  TELE TELEPHONE LINE
-  36" WM WATER LINE
-  CTV CABLE TV
-  TELE FO FIBER OPTICS
-  POWER TRANSMISSION LINE




R.O.W. PATTERNS

-  EX. LIMITED ACCESS R.O.W.
-  EXISTING R.O.W.
-  PROP LIMITED ACCESS R.O.W.
-  PROP FREE ACCESS R.O.W.
-  SECTION LINE





TOPO PATTERNS







-  HEDGE LINE
-  TREE LINE
-  EXISTING FENCE
-  PROPOSED FENCE
-  EXISTING GUARD RAIL
-  PROPOSED GUARD RAIL
-  DRAINAGE CRS/EDGE OF WATER
-  WETLANDS AREA
-  ABANDON ANY UTILITY
-  CITY LIMITS
-  RAILROAD
-  SOUND ABATEMENT WALL
-  CONCRETE MEDIAN BARRIER
-  SLOPE STAKE LINE

DRIVE/APPROACH LEGEND





-  CONCRETE
-  HMA
-  AGGREGATE

REMOVAL LEGEND

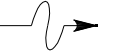
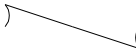
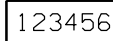

-  REMOVING HMA
-  REMOVING SIDEWALK
-  REMOVING PAVEMENT
-  COLD-MILLING

-  REMOVING CURB & GUTTER
-  REMOVING
-  ABANDONING
-  SAVE
-  BULKHEAD
-  CLEARING

SPECIAL LEGEND THIS PROJECT

-  Riprap, Plain
-  Erosion Control, Silt Fence
-  Erosion Control, Sediment Trap
-  DRAINAGE PATTERN

REAL ESTATE SYMBOLS

-  PROPERTY OWNERSHIP ARROW
-  CONTIGUOUS PROPERTY SYMBOL
-  123456 PARCEL NUMBER BOX
-  PARCEL LINES



LEGEND SHEET

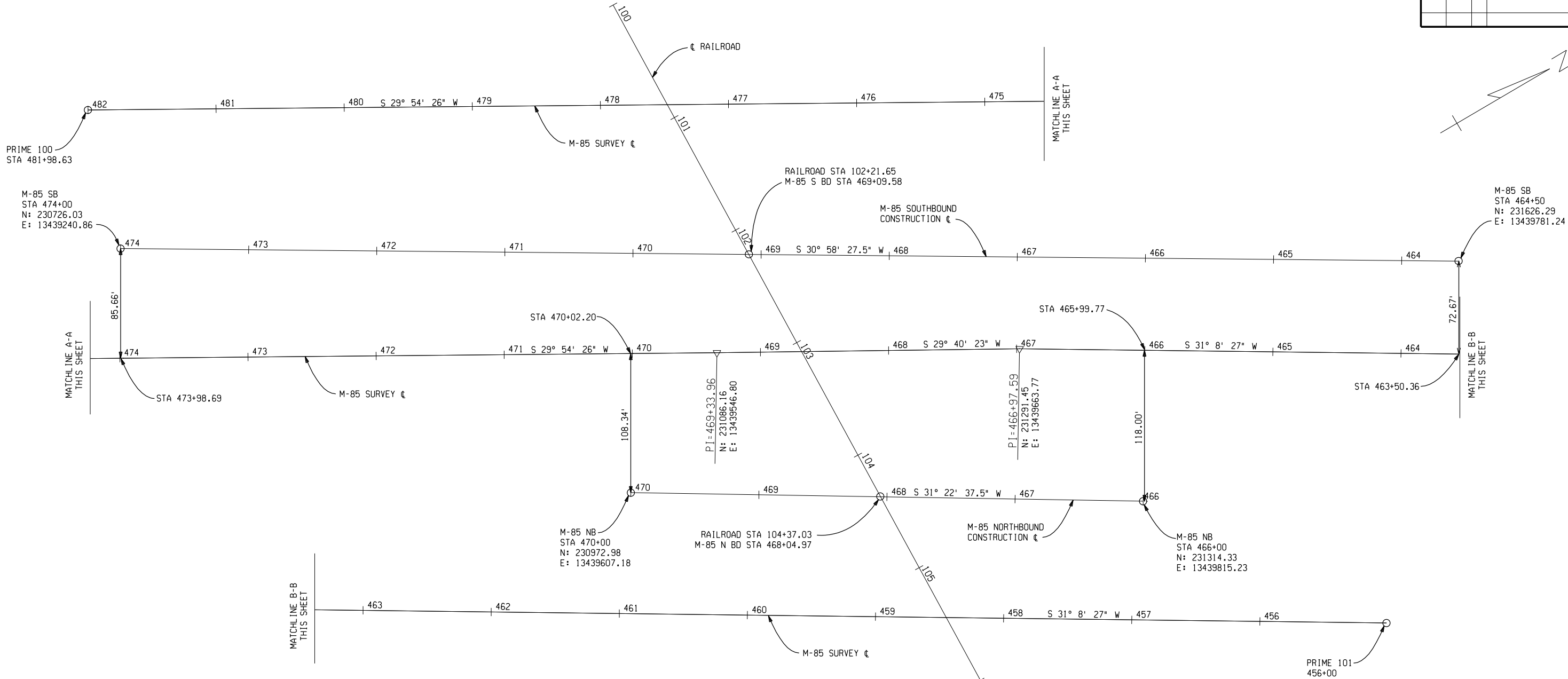
DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO.
03/10/06	NONE	82211	73160A	YUNG	R.O.W CONST. 6

CHECKED BY: ZK/CLG/EMK WORKED ON BY: EMK DATE: 4/22/04 DATE: 4/22/04

FINAL R.O.W.			
AUTH	DATE	NO.	REVISION

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DATE: 1/27/06
WORKED ON BY: SCW/MH
CHECKED BY: CLG/AS



PRIME 100

SET 5/8" x 36" IRON WITH SSI TRAVERSE POINT CAP
N. 62°E 44.28' TO NAIL AND SSI TAG IN SOUTHEAST FACE POWER POLE
N. 15°W 95.81' TO NAIL AND SSI TAG ON TOP OF GUARDRAIL POST - EAST SIDE OF SB FORT ROAD
N. 80°W 88.70' TO NAIL AND SSI TAG ON TOP OF GUARDRAIL POST - EAST SIDE OF SB FORT ROAD
N. 59°W 39.46' TO CENTER OF EASTERLY GUARDRAIL POST

DATUM: NAD83
N: 229989.92
E: 13438916.24
DATUM: NAVD88
EL: 582.21
S.F.: 0.999970853
(AVERAGE S.F.)

PRIME 101

SET 5/8" x 36" ROD WITH SSI TRAVERSE POINT CAP
S. 22°W 57.41' TO NAIL AND SSI TAG IN NORTHWEST FACE 8" RED PINE
DUE EAST 78.28' TO NAIL AND SSI TAG IN SOUTH FACE 10" RED PINE
N. 52°E 94.60' TO NAIL AND SSI TAG IN SOUTH FACE 8" LOCUST TREE
N. 45°W 46.83' TO EASTERLY EDGE OF BIT FOR SOUTHBOUND FORT ROAD

DATUM: NAD83
N: 232315.58
E: 13440282.55
DATUM: NAVD88
EL: 589.00
S.F.: 0.999970853
(AVERAGE S.F.)

CP #100

SET 5/8" x 36" IRON WITH SSI TRAVERSE POINT CAP
S. 48°W 54.79' TO NAIL AND SSI TAG IN N. FACE 16" BOX ELDER TREE
S. 05°W 62.34' TO NAIL AND SSI TAG IN N. FACE 12" BOX ELDER TREE
N. 45°E 12.49' TO SOUTHERLY BACK OF CURB FOR SOUTHBOUND AND NORTHBOUND TURN AROUND
N. 45°W 52.13' TO EASTERLY EDGE OF BIT FOR SOUTHBOUND FORT ROAD

DATUM: NAD83
NORTHING: 231291.45
EASTING: 13439663.77
DATUM: NAVD88
EL: 586.81
S.F.: 0.999970853
(AVERAGE S.F.)

CP #101

SET 5/8" x 36" IRON WITH SSI TRAVERSE POINT CAP
N. 58°E 10.01' TO NAIL AND SSI TAG IN SOUTH FACE POWER POLE
N. 71°E 54.11' TO SOUTHWEST CORNER OF RAILROAD ELECTRIC BUILDING
S. 72°W 79.38' TO CENTERLINE OF STOP AND ONE WAY SIGN POST
S. 70°E 96.62' TO CENTERLINE OF M-85 NORTH SIGN POST

DATUM: NAD83
NORTHING: 231086.16
EASTING: 13439546.79
DATUM: NAVD88
EL: 585.88
S.F.: 0.999970853
(AVERAGE S.F.)

BM #1

RAILROAD SPIKE IN NORTHEAST FACE POWER AND TELEPHONE POLE

N.45°W 16.58' TO PK NAIL IN BIT SIDEWALK
N.10°E. 6.20' TO NORTHERLY VAN HORN ROAD SIGN LEG
S.45°E. 8.70' TO WESTERLY EDGE BIT FOR SOUTHBOUND FORT ROAD
N.40°E. 55.28' TO CENTERLINE SOUTHERLY RAIL OF TRACKS

DATUM: NAVD88
EL 588.030
N 231098.2080
E 13439427.0037

BM #2

NORTH EASTERLY BOLT IN RAILROAD SIGN CANTILEVER FOR SOUTHBOUND FORT ROAD

N. 15°E. 29.82' TO NAIL AND SSI TAG IN EAST FACE POWER AND LIGHT POLE
DUE WEST 24.70' TO NAIL AND SSI TAG IN SOUTHEAST FACE POWER POLE FOR TRACKS
S. 28°W. 43.44' TO CENTERLINE NORTHERLY RAIL OF TRACKS
N. 45°W. 2.75' TO WESTERLY EDGE OF BIT OF SOUTHBOUND FORT ROAD

DATUM: NAVD88
EL 588.348
N 231187.5288
E 13439488.4560

BM #3

NORTHWESTERLY BOLT IN RAILROAD SIGN CANTILEVER BASE FOR NORTHBOUND FORT ROAD

S. 03°E. 12.16' TO NAIL AND SSI TAG IN NORTH FACE POWER AND LIGHT POLE
N. 32°E. 44.00' TO CENTERLINE SOUTHERLY RAIL OF TRACKS
N. 45°W. 2.00' TO EASTERLY EDGE BIT NORTHBOUND FORT ROAD

DATUM: NAVD88
EL 587.852
N 231098.2891
E 13439712.8936

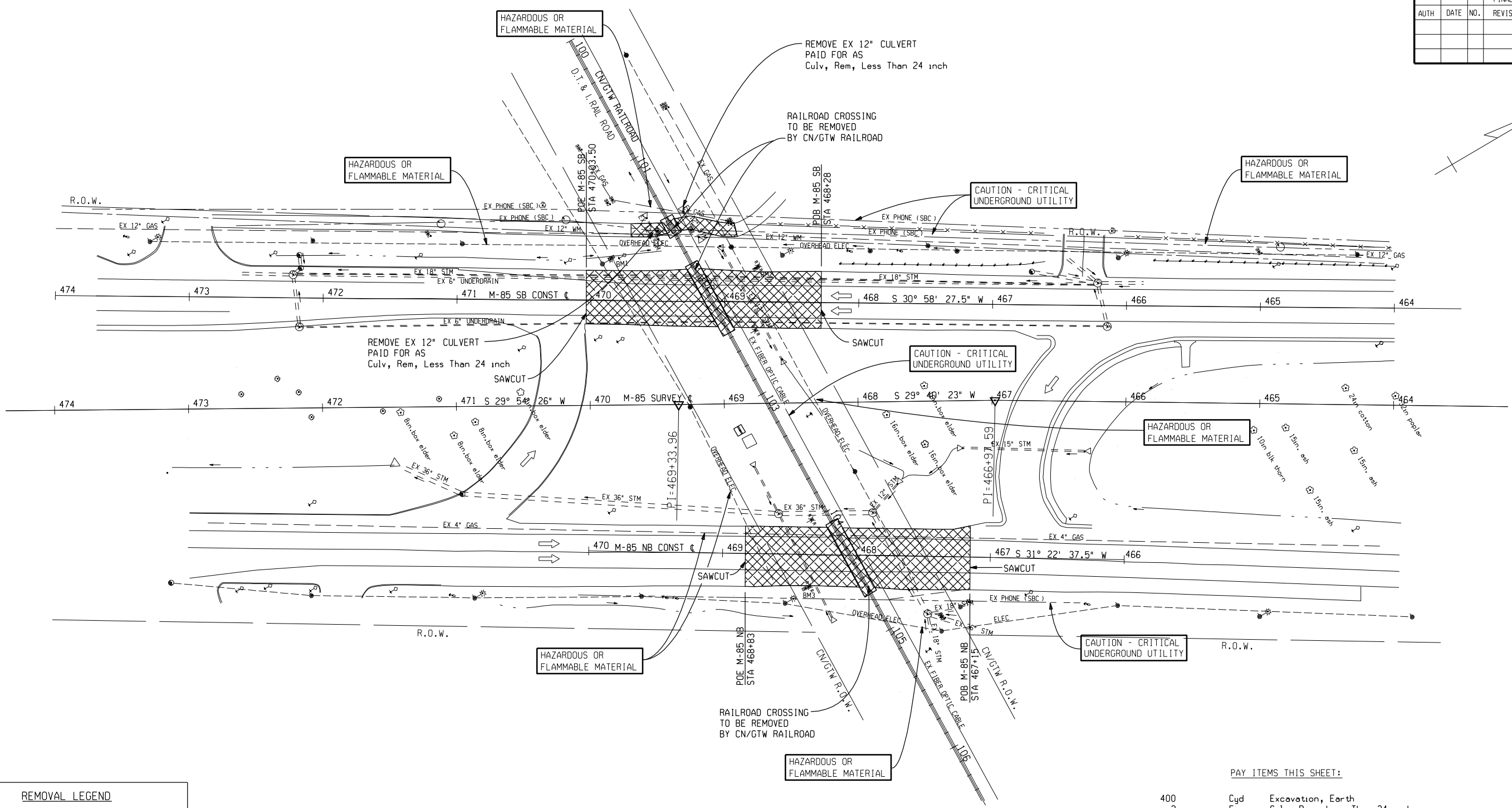
ALIGNMENT SHEET



M-85 AT GO1 OF 82211, CN/GTW RAILROAD CROSSING

DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO.
03/10/06	1" = 40'	82211	73160A	YUNG	R.O.W CONST.

FINAL R.O.W.			
AUTH	DATE	NO.	REVISION



REMOVAL LEGEND

REMOVING PAVEMENT

PAY ITEMS THIS SHEET:		
400	Cyd	Excavation, Earth
2	Ea	Culv, Rem, Less Than 24 inch
980	Syd	Pavt, Rem

NOTES:

REMOVAL OF RAILROAD CROSSING SURFACE, TRACK, HEADER BOARDS, CROSS TIES, AND ALL OTHER LABOR ASSOCIATED WITH THE REMOVAL OF SAID ITEMS WILL BE PERFORMED BY THE CN/GTW RAILROAD.

BALLAST REMOVAL AND EXCAVATION FOR RAILROAD CONSTRUCTION SHALL BE PERFORMED BY THE CN/GTW RR. FOR LIMITS OF EXCAVATION SEE SHEETS 3 & 4.

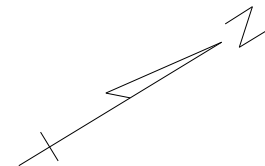
REMOVAL SHEET



M-85 AT GO1 OF 82211, CN/GTW RAILROAD CROSSING

DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO.
03/10/06	1" = 40'	82211	73160A	YUNG	R.O.W. CONST.




CHECKED BY: CIG/AS	DATE: 1/27/06	WORKED ON BY: SCW/MH	DATE: 1/27/06
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CONSTRUCTION OF RAILROAD CROSSING SURFACE,
EARTH EXCAVATION, AGGREGATE BASE, BALLAST,
UNDERDRAINS, HEADER BOARDS, CROSS TIES, GEOTEXTILE FABRIC,
AND ALL OTHER LABOR ASSOCIATED WITH THE INSTALLATION
OF SAID ITEMS SHALL BE PERFORMED BY THE CN/GTW RAILROAD.



FOR PAY ITEMS SEE NOTE SHEET

KEY

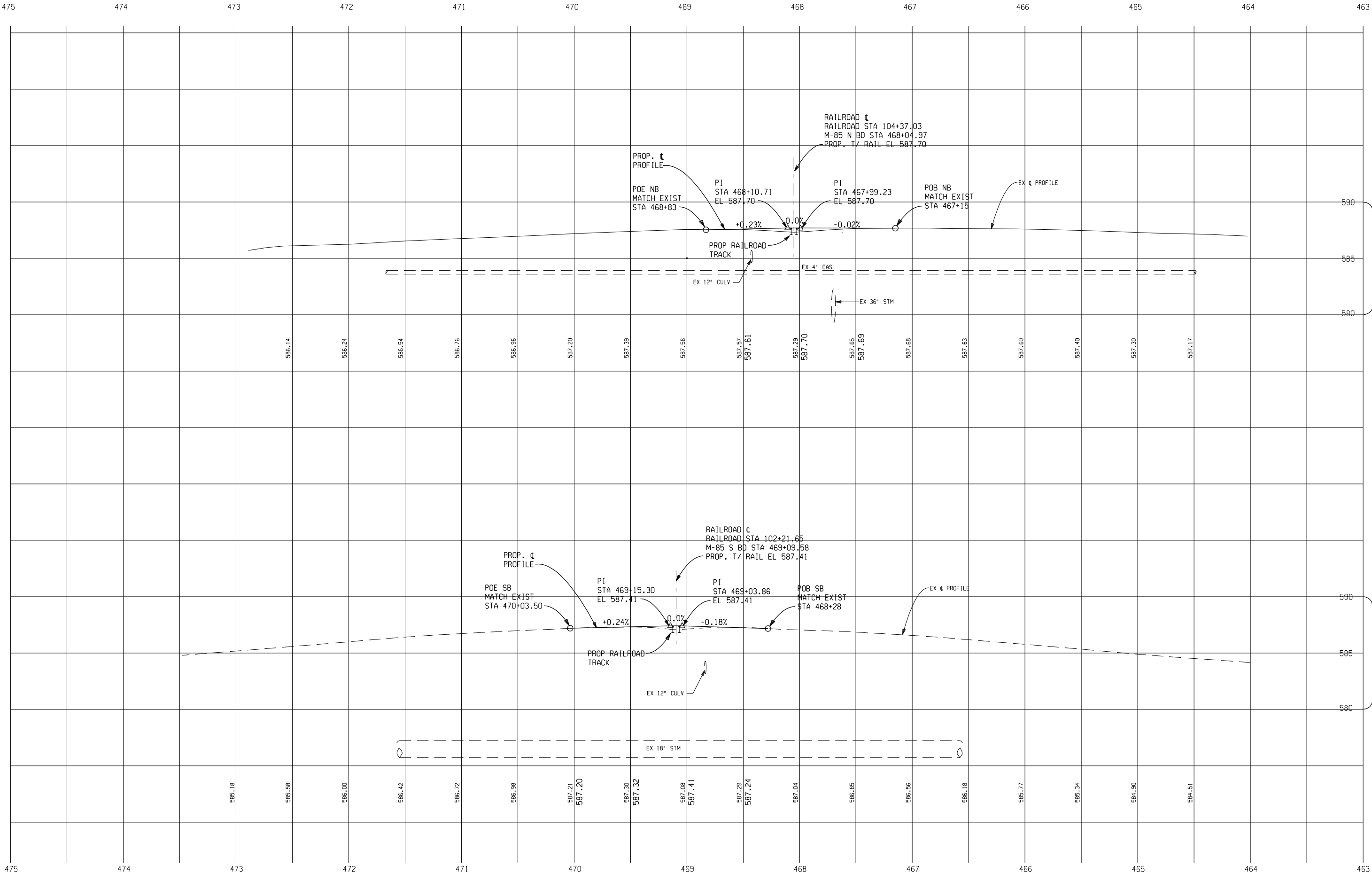
	Riprap, Plain
	Erosion Control, Silt Fence
	Erosion Control, Sediment Trap

50	Cyd	Embankment, CIP
10	Cyd	Subbase, CIP
393	Ton	Aggregate Base
203	Ton	HMA, 4C
683	Ton	HMA, 3C
4	Ea	Culv End Sect, Conc, 12 inch
58	Ft	Culv, Cl A, Conc, 12 inch

M-85 AT GO1 OF 82211, CN/GTW RAILROAD CROSSING

<div><div>ISO 9001 CERTIFIED</div><div></div><div>ENGINEERING & SURVEYING EXCELLENCE SINCE 1954</div></div>	<div>MDOT Michigan Department of Transportation</div>	M-85 AT GO1 OF 82211, CN/GTW RAILROAD CROSSING					
		DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO. R.O.W
		03/10/06	1" = 40'	82211	73160A	YUNG	

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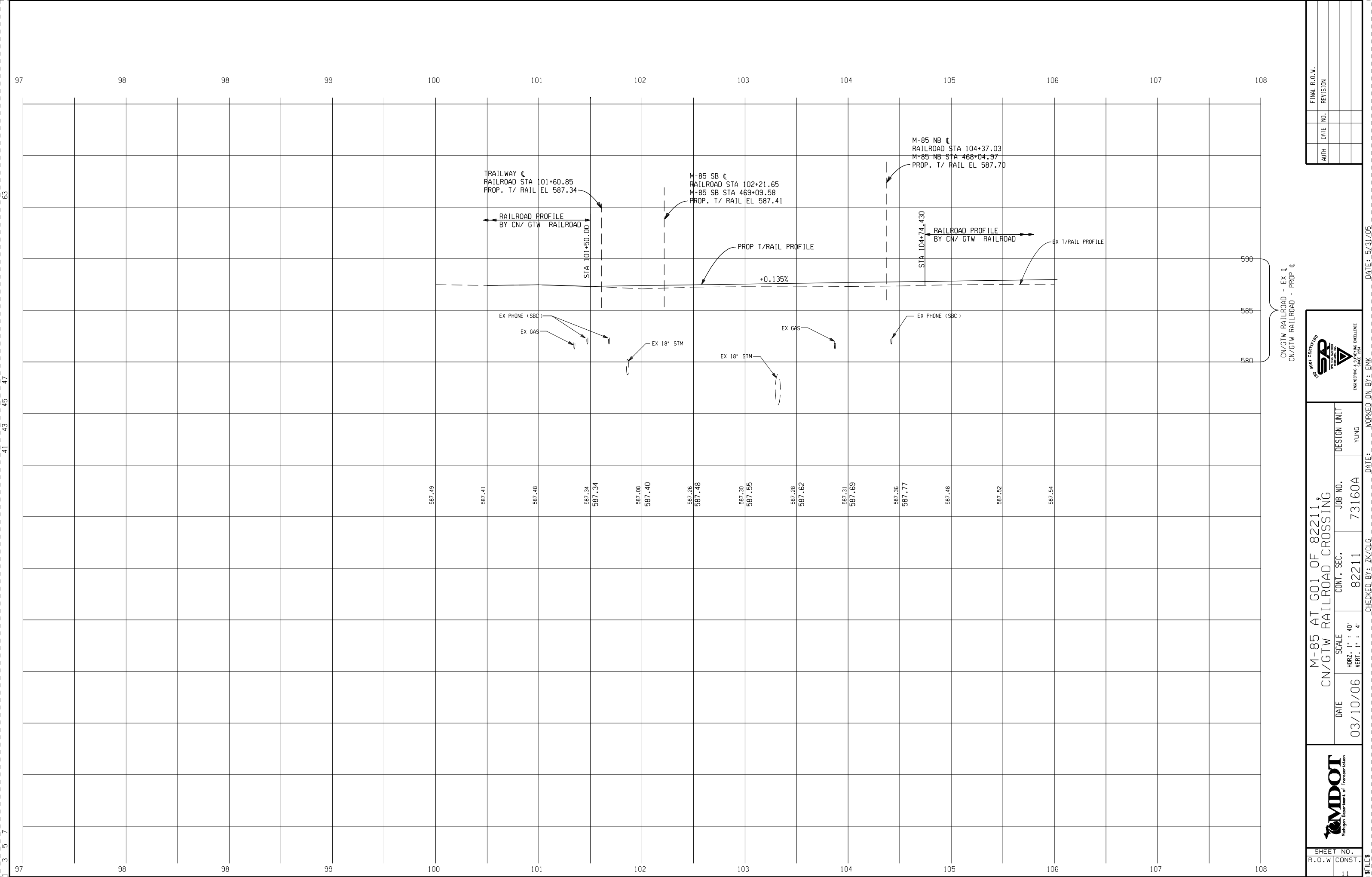
SHEET NO.		R.O.W CONST.	
10			

MDOT Michigan Department of Transportation		M-85 AT G01 OF 82211, CN/GTW RAILROAD CROSSING	
DATE	SCALE	CONT. SEC.	JOB NO.
03/10/06	HORIZ. 1" = 40' VERT. 1" = 4'	82211	73160A
DESIGN UNIT		YUNG	

ISO 9001 CERTIFIED		ENGINEERING & SURVEYING EXCELLENCE SINCE 1954	
SA SINCE 1954			

FINAL R.O.W.	REVISION	AUTH	DATE

WORKED ON BY: EMK
DATE: 5/31/05
CHECKED BY: ZK/CLG
DATE:



FINAL R.O.W.		REVISION	
AUTH	DATE	NO.	



M-85 AT G01 OF 82211, CN/GTW RAILROAD CROSSING				DESIGN UNIT	
DATE	SCALE	CONT. SEC.	JOB NO.	YOUNG	
03/10/06	HORIZ. 1" = 40' VERT. 1" = 4'	82211	73160A		



SHEET NO.	CONST.
R.O.W	

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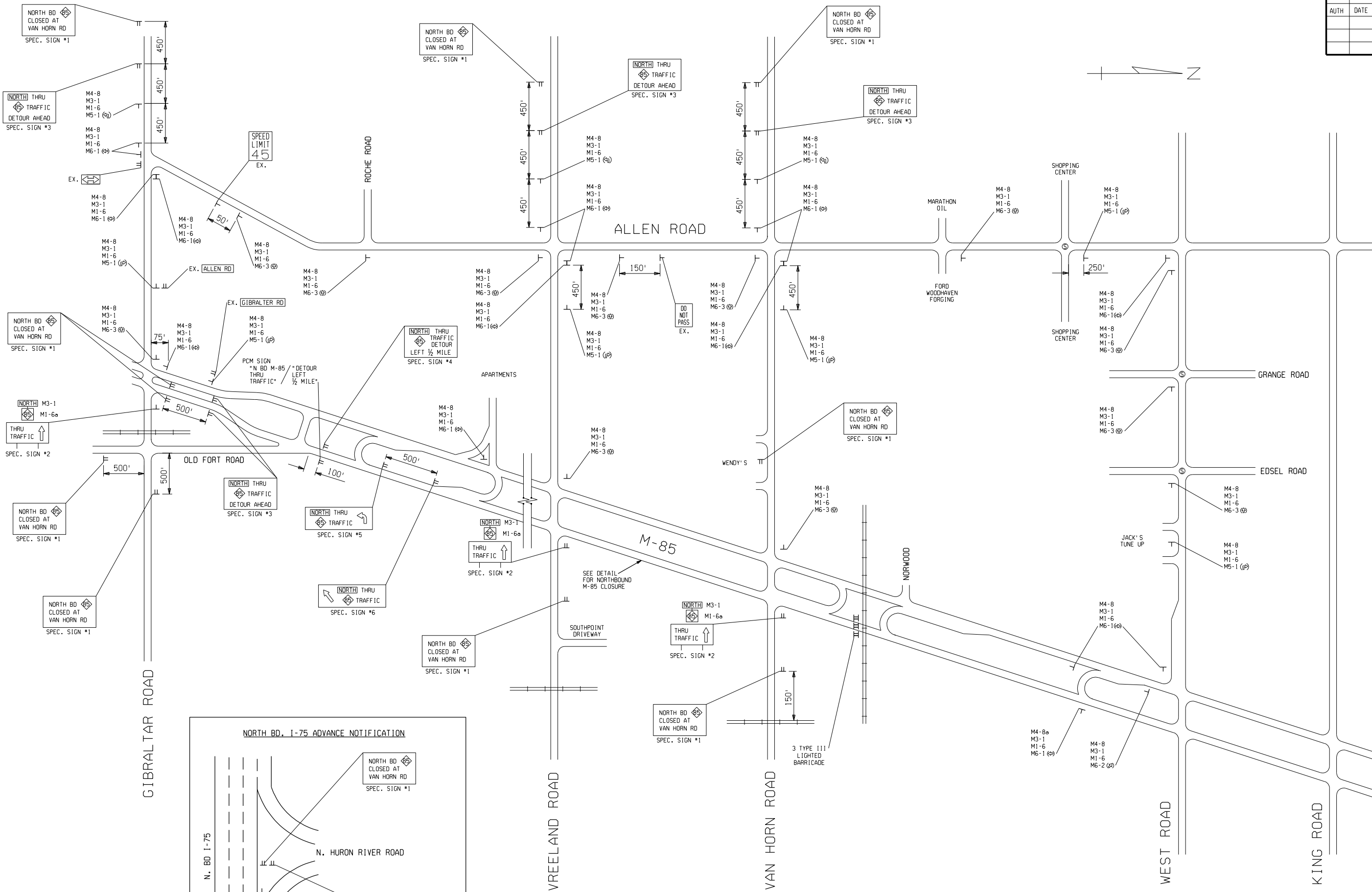
DATE: 5/31/05

WORKED ON BY: EMK

CHECKED BY: ZK/CLG

DATE:

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STAGE 1 - DETOUR FOR NORTHBOUND M-85 CLOSURE

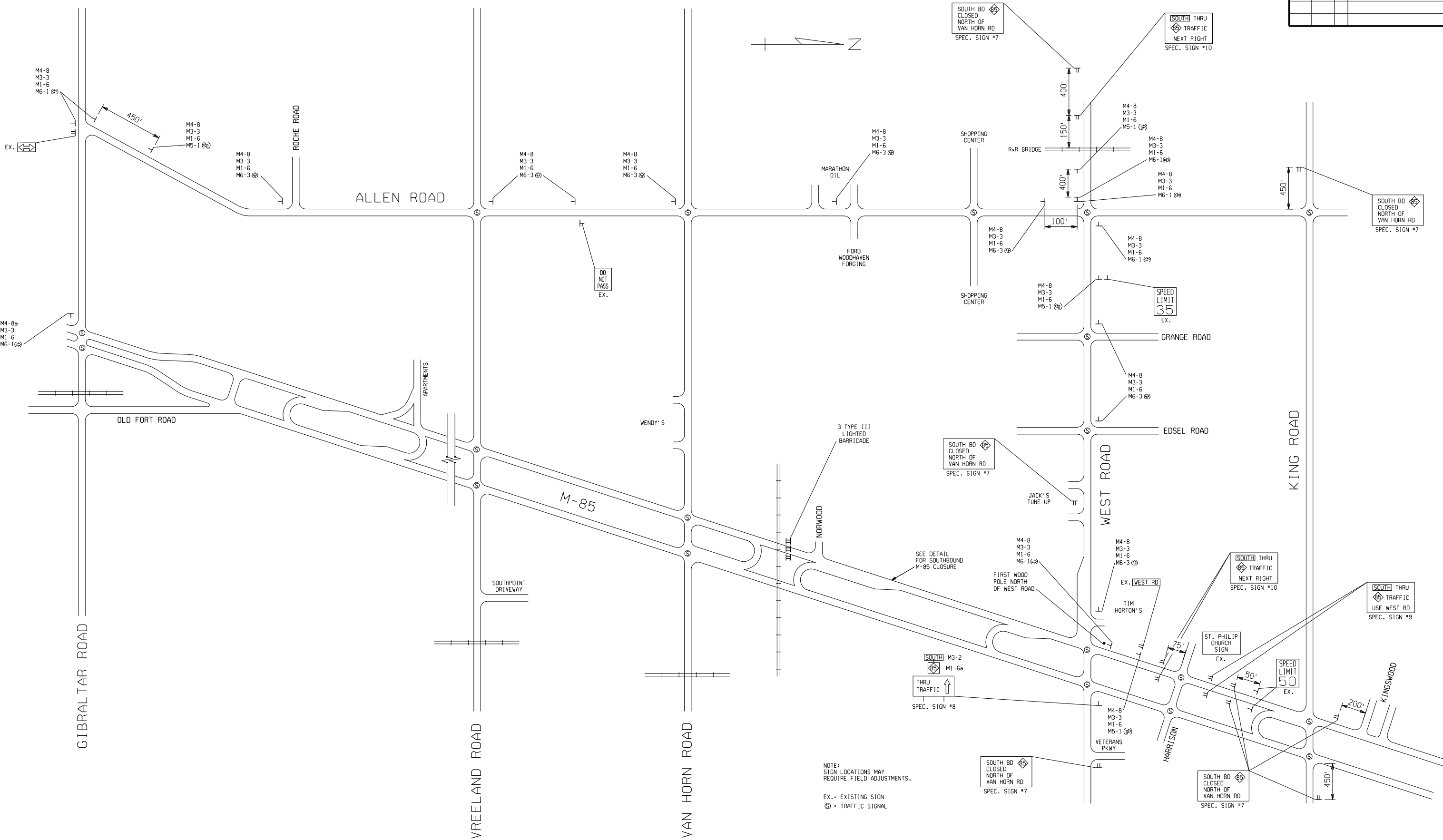
NOTE:
SIGN LOCATIONS MAY
REQUIRE FIELD ADJUSTMENTS.
EX. = EXISTING SIGN
S = TRAFFIC SIGNAL



M-85 AT GO1 OF 82211, CN/GTW RAILROAD CROSSING

DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO.
03/10/06	NONE	82211	73160A	YUNG	R.O.W CONST. 12

1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16 17 18 19 20 21 22 23 24 25 26 27 28 29 30 31 32 33 34 35 36 37 38 39 40 41 42 43 44 45 46 47 48 49 50 51 52 53 54 55 56 57 58 59 60 61 62 63



FINAL R.O.W.			
AUTH	DATE	NO.	REVISION

DATE: 1/27/06
WORKED ON BY: SCW/MH
CHECKED BY: CLG/AS

STAGE 2 - DETOUR FOR SOUTHBOUND M-85 CLOSURE



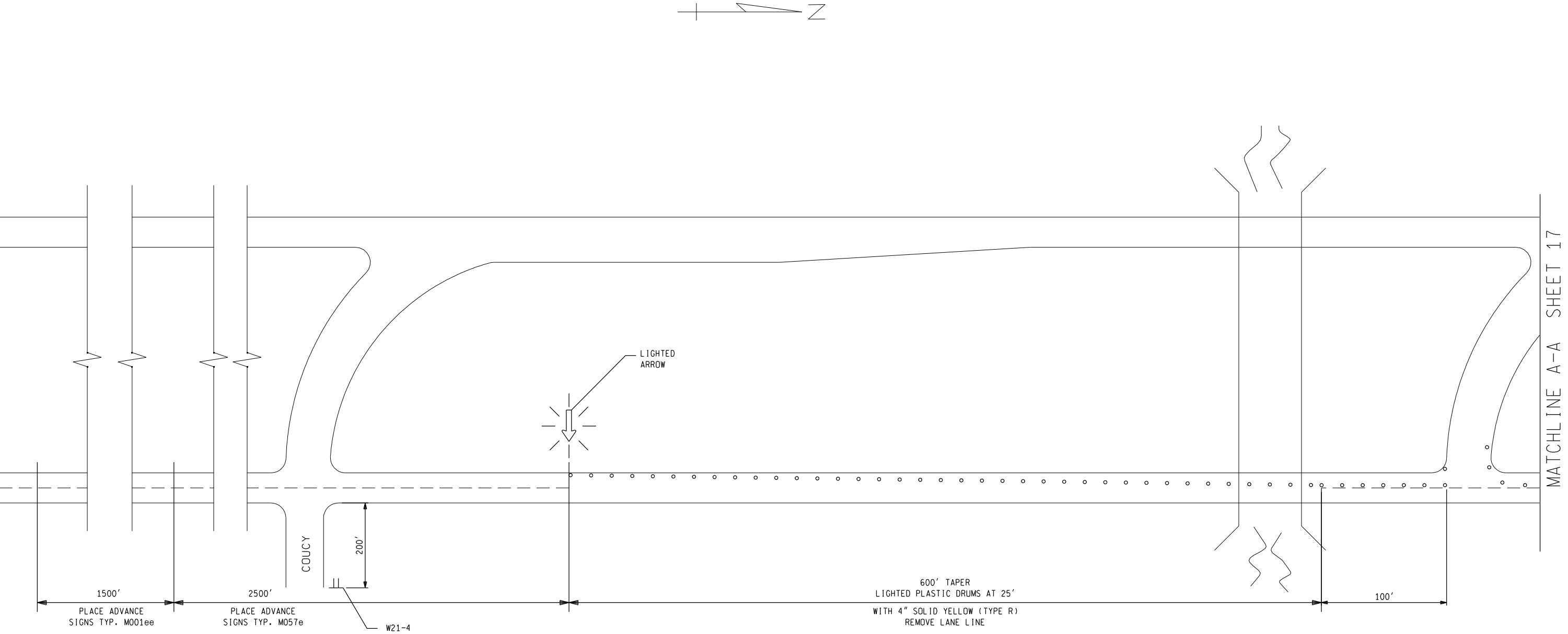
M-85 AT GO1 OF 82211, CN/GTW RAILROAD CROSSING

DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO.
03/10/06	NONE	82211	73160A	YUNG	15

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FINAL R.O.W.			
AUTH	DATE	NO.	REVISION

DATE: 1/27/06
WORKED ON BY: SCW/MH
CHECKED BY: CLG/AS



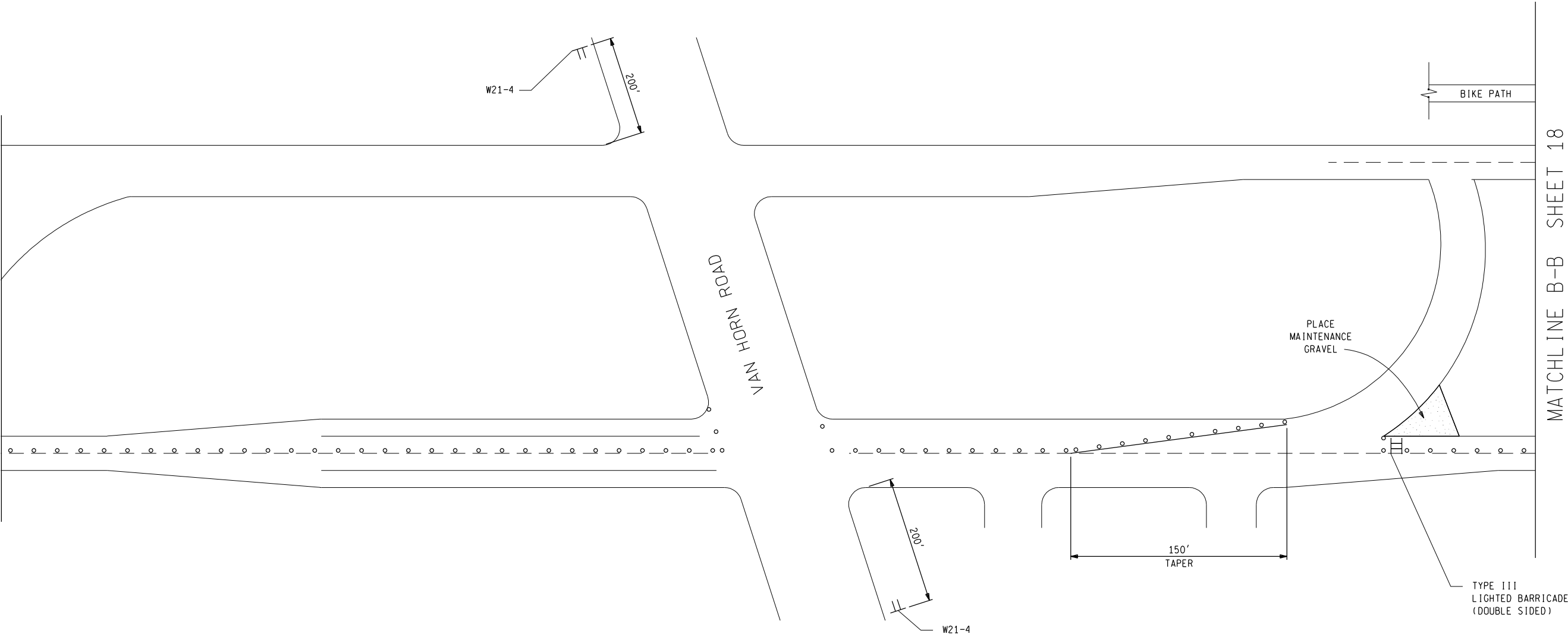
NOTE:
SIGN LOCATIONS MAY
REQUIRE FIELD ADJUSTMENTS.

STAGE 2 SOUTHBOUND M-85 CLOSURE DETAIL

	M-85 AT GO1 OF 82211, CN/GTW RAILROAD CROSSING									
	DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO.				
	03/10/06	1"=40'	82211	73160A	YUNG	R.O.W	CONST.	16	FILES	

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MATCHLINE A-A SHEET 16



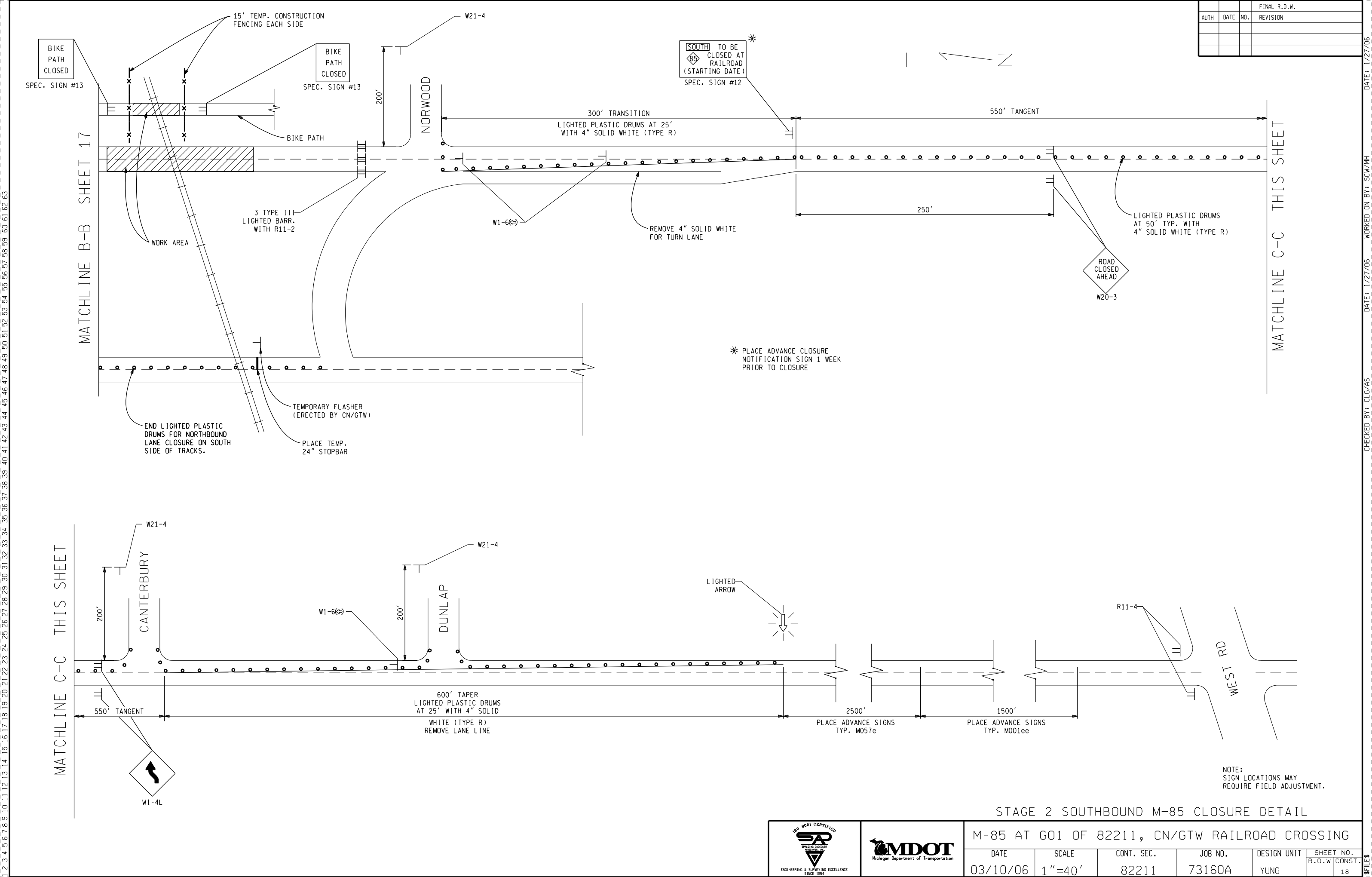
NOTE:
SIGN LOCATIONS MAY
REQUIRE FIELD ADJUSTMENTS.

STAGE 2 SOUTHBOUND M-85 CLOSURE DETAIL

<div> <div>150 9001 CERTIFIED</div> <div> </div> <div> <div>ENGINEERING & SURVEYING EXCELLENCE</div> <div>SINCE 1964</div> </div> </div>	<div> <div>MDOT</div> <div>Michigan Department of Transportation</div> </div>	M-85 AT GO1 OF 82211, CN/GTW RAILROAD CROSSING					SHEET NO.	
		DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT	R.O.W	CONST.
		03/10/06	1"=40'	82211	73160A	YUNG		17

FINAL R.O.W.			
AUTH	DATE	NO.	REVISION

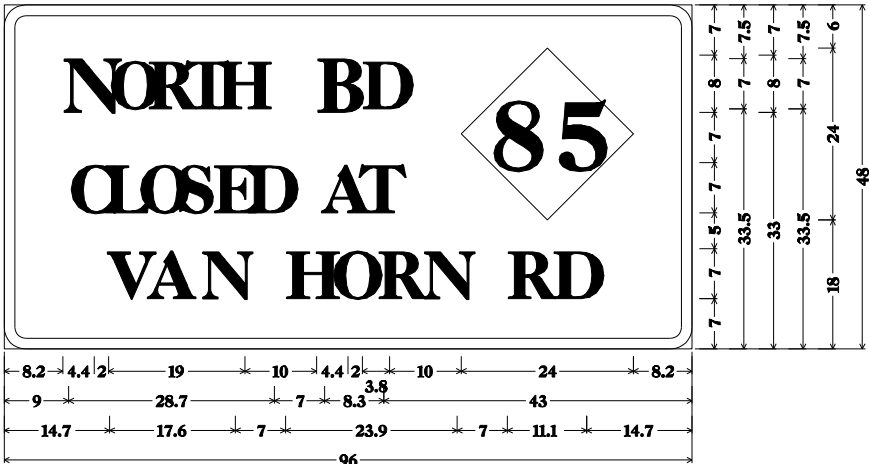
DATE: 1/27/06
WORKED ON BY: SCW/MH
CHECKED BY: CLG/AS
FILES



FINAL R.O.W.			
AUTH	DATE	NO.	REVISION

DATE: 1/27/06
WORKED ON BY: SCW/MH
CHECKED BY: CLG/AS
FILES

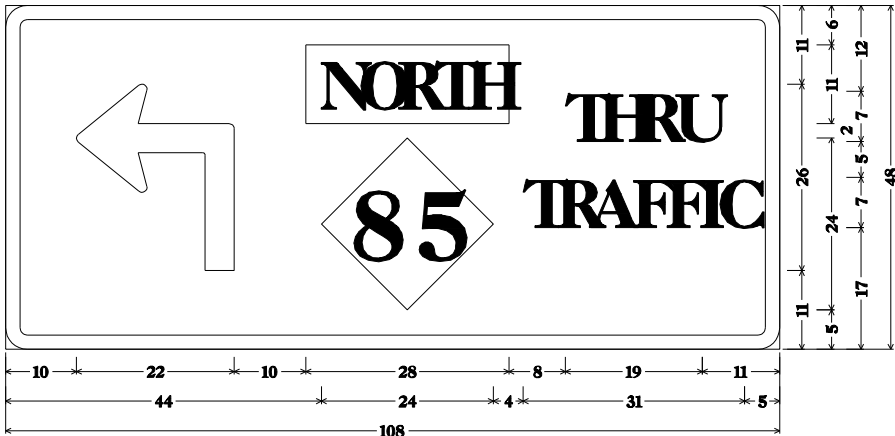
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Special Sign #1; 3.0" Radius, 1.5" Border, Black on Orange;
[N ORTH] C; [B D] C; [CLOSED AT] C; State Highway 85 MI-6a;
[VAN HORN RD] D;

Table of widths and spaces.

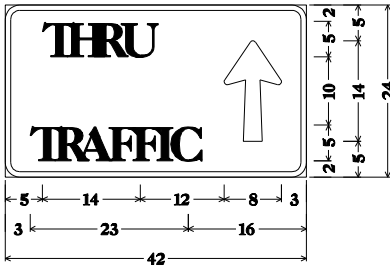
8.2	N	4.4	2.0	O	1.5	R	3.8	1.2	T	3.5	1.2	H	3.8	10.0	B	4.4	2.0	D	3.8	10.0	Φ	24.0	8.2	
C	3.8	L	3.5	O	4.0	S	3.8	1.5	E	3.5	1.2	D	3.8	7.0	A	4.4	0.4	T	3.5	43.0				
9.0	3.8	1.2	3.5	1.2	4.0	1.2	3.8	1.5	3.5	1.2	3.8	7.0	4.4	0.4	3.5	43.0								
V	A	N	H	O	R	L	E	D	A	T														
14.7	5.3	0.4	5.9	L3	N	4.7	7.0	4.7	1.6	O	4.9	1.7	R	4.7	1.6	N	4.7	7.0	R	4.7	1.7	D	4.7	14.7



Special Sign #5; 3" Radius, 2" Border, Black on Orange;
90 Deg Advanced Turn Arrow Custom 22" X 26"; Rectangle White;
State Highway 85 MI-6a; [THRU] C; [TRAFFIC] C;

Table of widths and spaces.

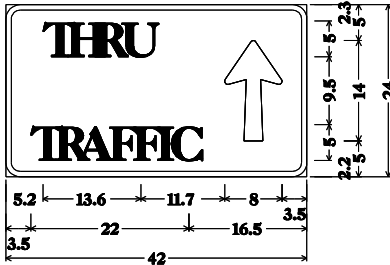
10	Φ	22	10	28	8	T	4	1	H	4	1	R	4	1	U	4	11							
44	24	4	4	1	4	1	4	1	4	1	4	1	4	1	4	1	1	1	1	1	1	1	1	5



Special Sign #8;
2" Radius, 1" Border, Black on Orange;
[THRU] C; [TRAFFIC] C;
Arrow B-8 - 14" 90°;

Table of widths and spaces.

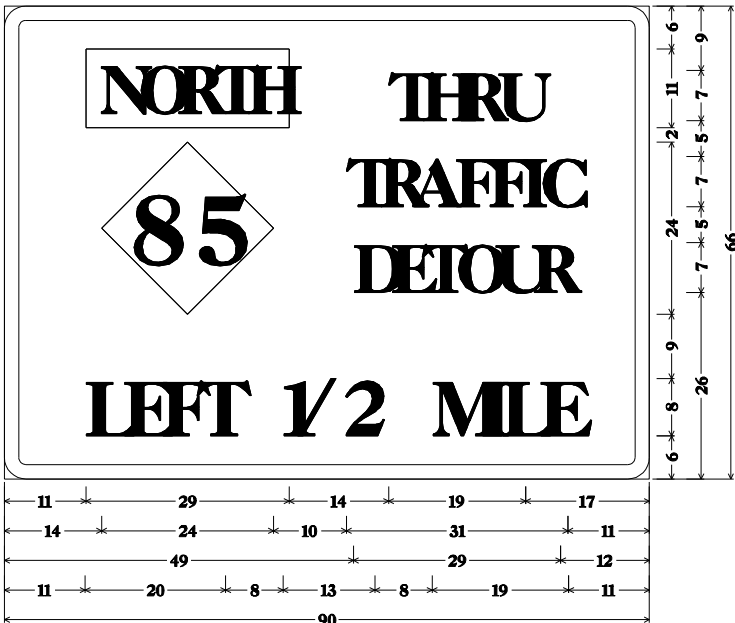
5	T	3	1	H	2	1	R	3	1	U	3	12	8	3										
3	T	3	1	R	3	0	A	4	0	F	3	1	F	2	1	I	1	1	1	C	3	16		



Special Sign #2;
2.0" Radius, 0.8" Border, Black on Orange;
[THRU] C; [TRAFFIC] C;
Arrow B-8 - 14.0" 90°;

Table of widths and spaces.

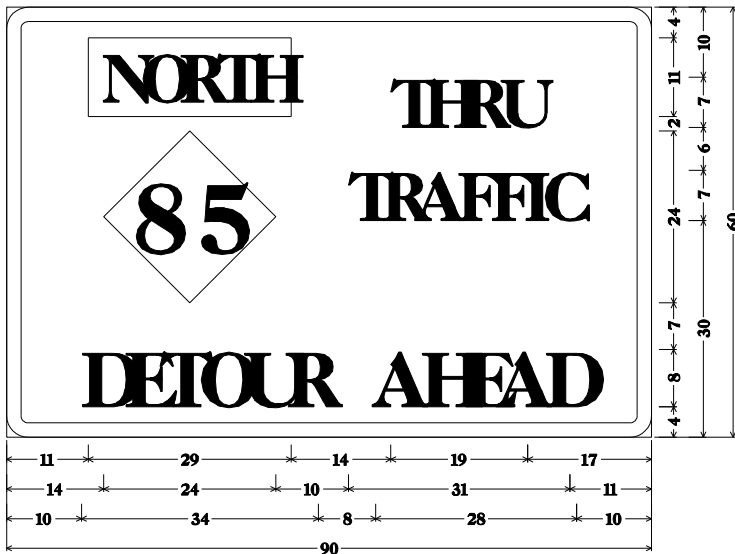
Table of widths and spaces.																					
T	H	U																			
5.2	2.5	0.8	2.8	1.0	R	2.7	1.1	2.7													
			↑																		
		11.7	8.0	3.5																	
3.5	T	2.5	0.8	R	2.7	0.9	A	3.1	0.9	F	2.5	0.8	F	2.5	0.8	I	0.7	1.1	C	2.7	16.5



EL-1 (L5) I; 3" Radius, 2" Border, Black on Orange;
Rectangle White;
State Highway 85 MI-6a; [THRU] C; [TRAFFIC] C; [DETOUR] C;
[LEFT 1/2 MILE] C;

Table of widths and spaces.

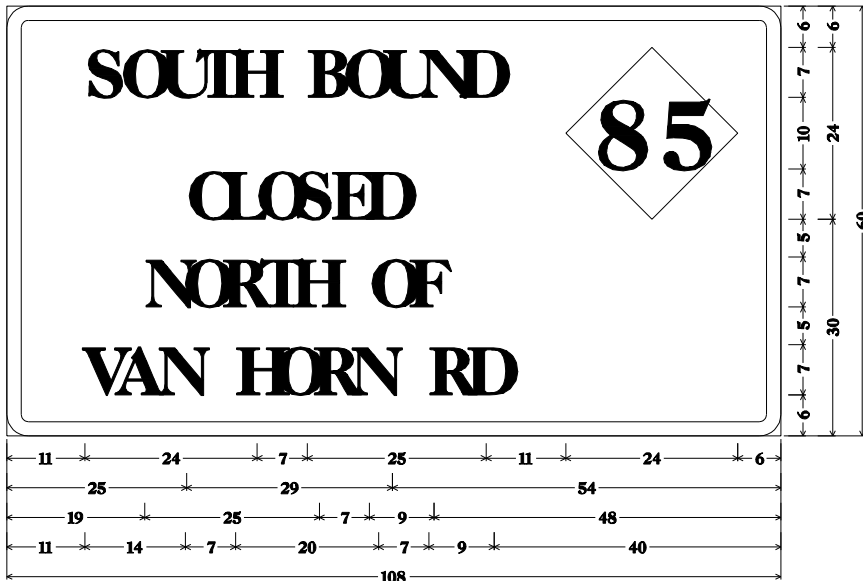
11	29	14	3	1	4	2	3	2	4	17														
14	Φ	10	T	1	R	A	5	F	3	2	F	3	1	I	1	2	C	4	11					
	24	3	3																					
	D		E		T	O		U		R														
49	4	1	4	0	4	1	4	2	3	2	4	12												
	L		E		F	1	T		1	/		2			M		I		L		E			
11	4	2	4	E	1	4	1	4	8	2	-0	6	0	2	5	8	5	2	1	1	4	2	4	11



Special Sign #3; 3" Radius, 2" Border, Black on Orange;
Rectangle White;
State Highway 85 MI-6a; [THRU] C; [TRAFFIC] C; [DETOUR AHEAD] C;

Table of widths and spaces.

11	29	14	3	1	4	2	3	2	4	17																							
14	Φ	24	10	T	3	1	R	4	1	A	5	1	F	3	2	F	3	1	I	1	2	C	4	11									
10	D	5	2	E	4	0	T	4	2	O	4	2	U	4	2	R	5	8	A	5	1	H	4	2	E	4	1	A	5	1	D	5	10



Special Sign #7; 3" Radius, 2" Border, Black on Orange;
[SOUTH BOUND] C; [CLOSED] C; [NORTH OF] C; [VAN HORN RD] C;
State Highway 85 MI-6a;

Table of widths and spaces.

11	4	1	4	1	4	1	4	1	4	7	4	1	4	1	4	2	4	1	4	11	24	6		
25	C	1	L	O	1	S	1	E	1	D	54													
19	N	4	2	O	1	R	4	1	T	4	H	7	O	4	1	F	4	48						
11	V	4	1	A	1	N	4	7	H	4	O	2	R	4	1	N	4	7	R	4	1	D	4	40

SPECIAL SIGNS

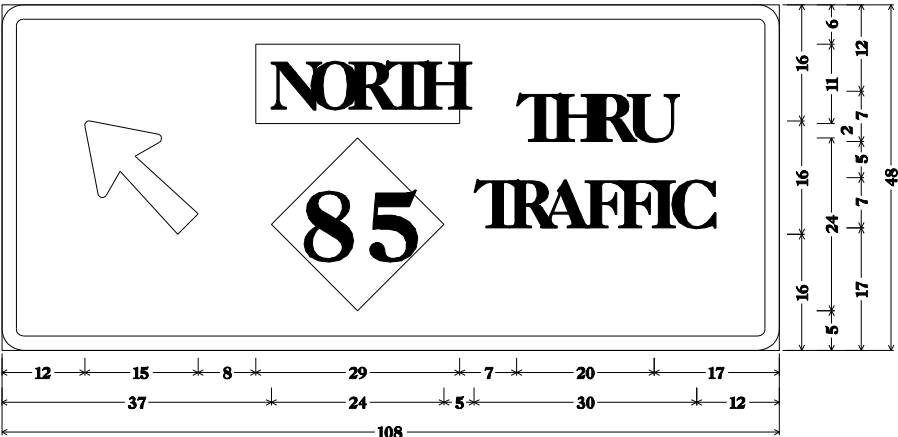
				M-85 AT G01 OF 82211, CN/GTW RAILROAD CROSSING							
DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO.	R.O.W	CONST.				
03/10/06	NONE	82211	73160A	YUNG	19						

DATE: 1/27/06 WORKED ON BY: SCW/MH

FILES

			FINAL R.O.W.
AUTH	DATE	NO.	REVISION

1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16 17 18 19 20 21 22 23 24 25 26 27 28 29 30 31 32 33 34 35 36 37 38 39 40 41 42 43 44 45 46 47 48 49 50 51 52 53 54 55 56 57 58 59 60 61 62 63






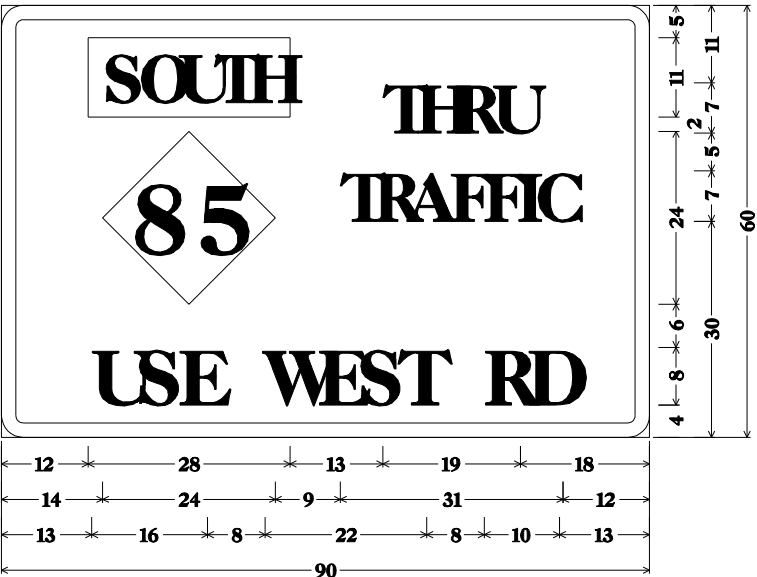
Special Sign #6; 3" Radius, 2" Border, Black on Orange;

Arrow B-12 - 20" 135{; Rectangle White;

State Highway 85 MI-6a; [THRU] C; [TRAFFIC] C;

Table of widths and spaces.

12		8		7	T	4	1	H	4	1	R	4	2	U	4	17							
37		5	T	3	1	R	4	1	A	5	1	F	3	2	F	3	1	I	1	2	C	3	12



Special Sign #9; 3" Radius, 2" Border, Black on Orange;

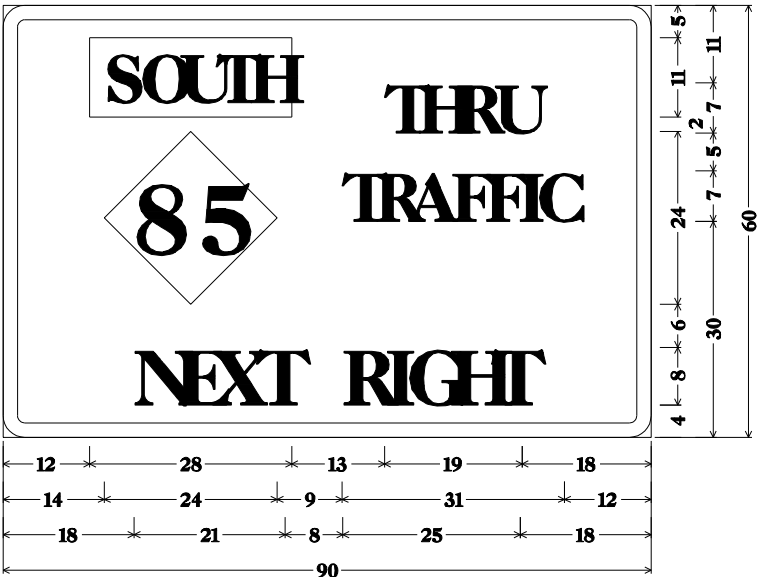
Rectangle White;

State Highway 85 MI-6a; [THRU] C; [TRAFFIC] C;

[USE WEST RD] C;

Table of widths and spaces.

12	<div> </div>	28	13	T	3	2	H	3	2	R	4	1	U	4	18												
14	<div> </div>	24	9	T	4	1	R	4	1	A	4	1	F	4	1	F	4	1	I	1	1	C	4	12			
13	U	4	2	S	4	2	E	4	8	W	6	1	E	4	1	S	5	1	T	4	8	R	4	2	D	4	13



Special Sign #10; 3" Radius, 2" Border, Black on Orange;

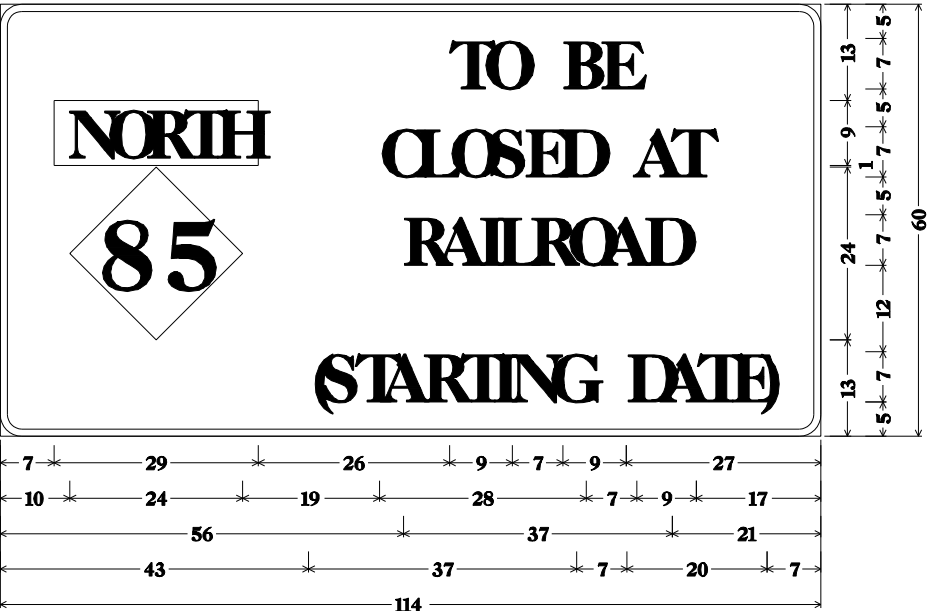
Rectangle White;

State Highway 85 MI-6a; [THRU] C; [TRAFFIC] C;

[NEXT RIGHT] C;

Table of widths and spaces.

12	<div>□</div> 28	13	T	3	2	H	3	2	R	4	1	U	4	18													
14	<div>⬢</div> 24	9	T	4	1	R	4	1	A	4	1	F	4	1	F	4	1	I	1	1	C	4	12				
18	N	5	1	E	4	2	X	4	1	T	4	8	R	5	1	I	1	2	G	4	2	H	4	2	T	4	18



Special Sign #11; 3" Radius, 1" Border, Black on Orange;

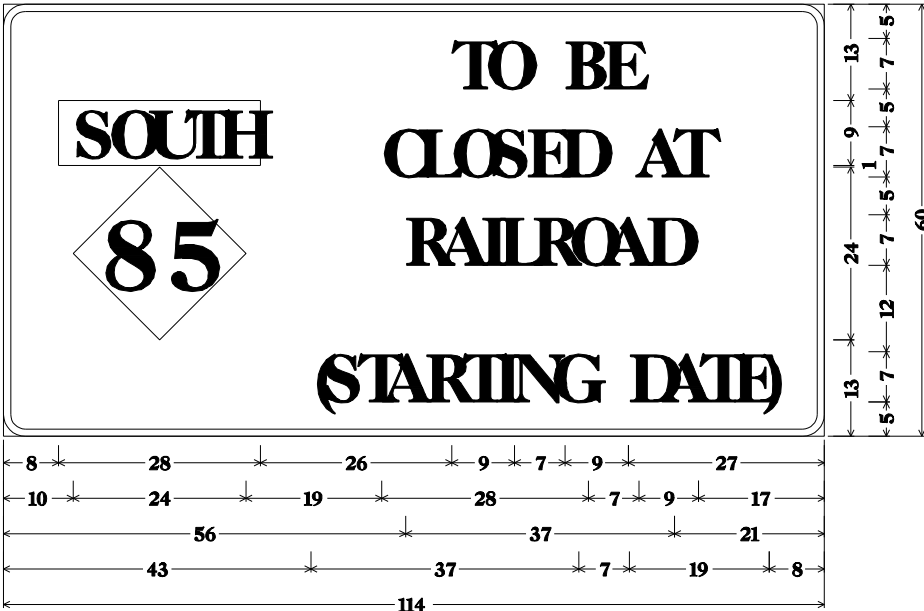
Rectangle White;

State Highway 85 MI-6a; [TO BE] C; [CLOSED AT] C; [RAILROAD] C;

[(STARTING DATE)] C;

Table of widths and spaces.

7	□		T		O		B		E									
	29	26	4	1	4	7	4	1	4	27								
10	♠		C		L		O		S		E		D		A		T	
	24	19	3	2	3	1	4	2	3	2	3	2	3	7	5	0	4	17
56	R		A		I		L		R		O		A		D			
	4	1	4	2	1	1	4	1	4	1	4	1	4	2	3	21		
43	(		S		T		A		R		T		I		N		G	
	1	0	4	1	4	0	5	1	4	1	3	1	1	2	4	1	4	
		D		A		T		E)								
	7	4	1	4	1	3	2	3	0	2	7							



Special Sign #12; 3" Radius, 1" Border, Black on Orange;

Rectangle White;

State Highway 85 MI-6a; [TO BE] C; [CLOSED AT] C; [RAILROAD] C;

[(STARTING DATE)] C;

Table of widths and spaces.

8	<div>□</div> 28	26	T	4	1	O	4	7	B	4	1	E	4	27												
10	<div>⚡</div> 24	19	C	3	2	L	3	1	O	4	1	S	4	2	E	3	1	D	4	7	A	5	0	T	4	17
56	R	4	1	A	4	1	I	1	2	L	3	2	R	3	2	O	4	1	A	4	1	D	4	21		
43	(	0	S	4	1	T	4	0	A	4	2	R	3	2	T	3	1	I	1	2	N	4	1	G	4	
		7	D	4	1	A	4	1	T	3	1	E	4	-0	)	1	8									



Special Sign #13;

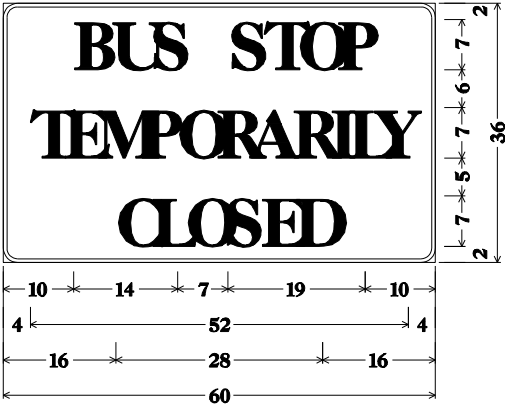
2" Radius, 1" Border, Black on Orange;

[BIKE] C; [PATH] C;

[CLOSED] C;

Table of widths and spaces.

10	B	1	I	2	K	2	E	10		16	3							
	4		1		1		3					3						
9	P	4	1	A	4	1	T	3	1	H	4	9						
4	C	3	2	L	3	1	O	4	2	S	3	2	E	3	2	D	3	4



Special Sign #14;

2" Radius, 1" Border, Black on Orange;

[BUS STOP] C; [TEMPORARILY] C;

[CLOSED] C;

Table of widths and spaces.

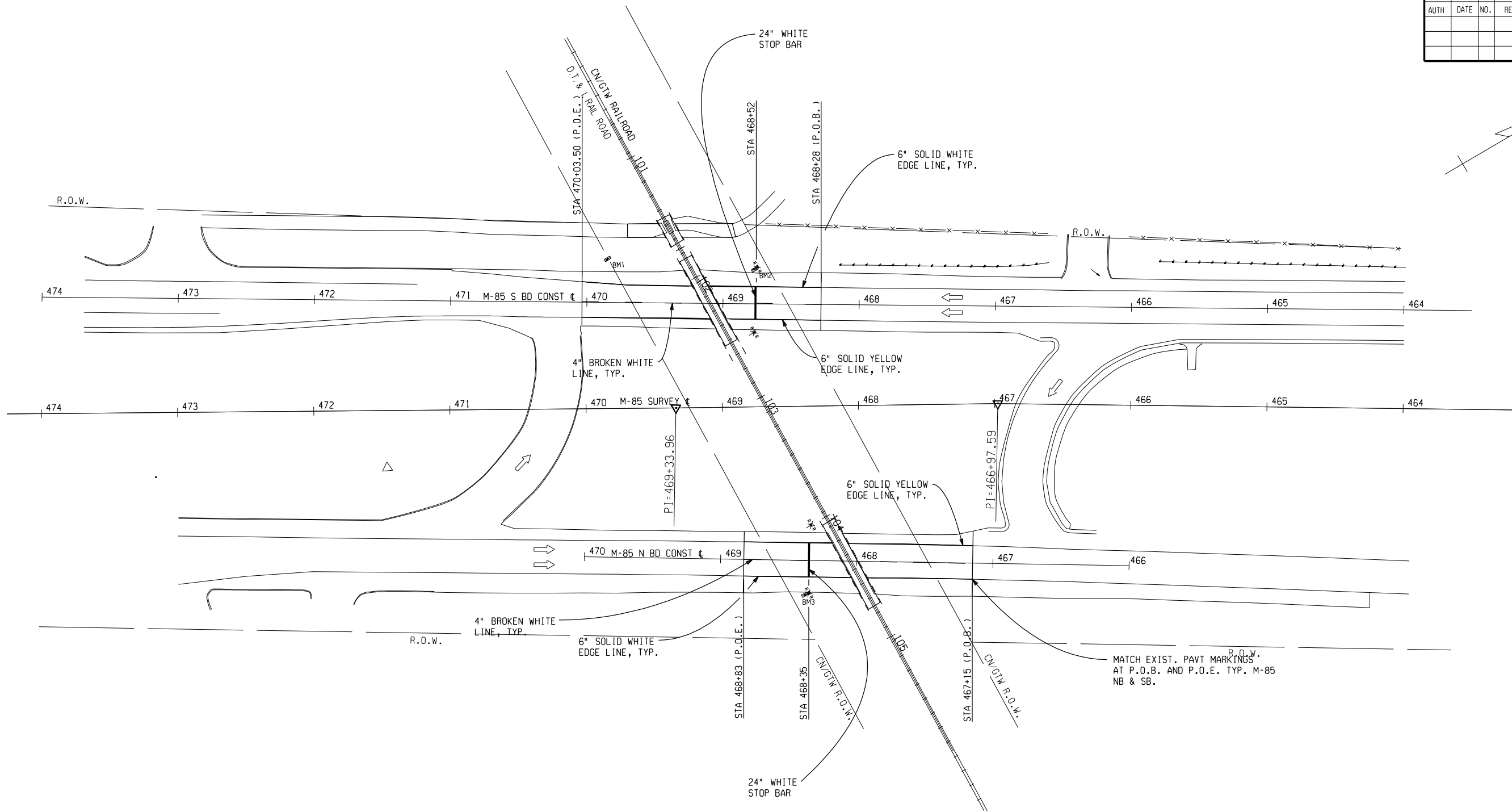
10	B	4	1	U	4	1	S	4																									
			7	S	4	1	T	4	1	O	4	1	P	4	10																		
4	T	3	1	E	4	1	M	5	1	P	4	1	O	4	2	R	3	2	A	4	1	R	4	2	I	1	1	L	3	1	Y	4	4
16	C	3	2	L	3	1	O	4	2	S	3	2	E	3	2	D	3	16															

SPECIAL SIGNS

		M-85 AT GO1 OF 82211, CN/GTW RAILROAD CROSSING							
		DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO.	R.O.W	CONST.
		03/10/06	NONE	82211	73160A	YUNG	20		

AUTH	DATE	NO.	FINAL R.O.W.	REVISION

FINAL R.O.W.			
AUTH	DATE	NO.	REVISION



NOTE:
THE CONTRACTOR SHALL LOG EXISTING PERMANENT PAVEMENT MARKINGS TO BE REMOVED FOR MAINTAINING TRAFFIC AND RESTORE THEM IN-KIND AS DIRECTED BY THE ENGINEER INCLUDED IN PAYMENT FOR PERMANENT PAVEMENT MARKING PAY ITEMS.

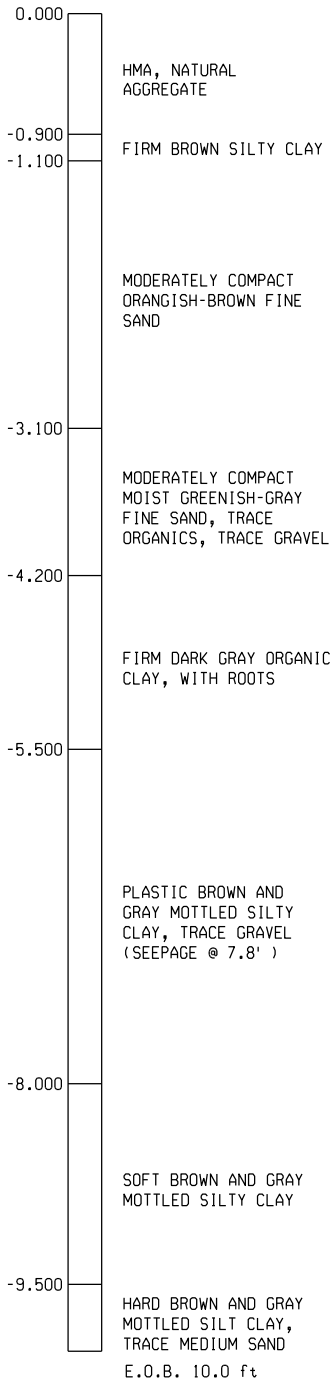
PERMANENT PAVEMENT MARKING SHEET

		M-85 AT G01 OF 82211, CN/GTW RAILROAD CROSSING					
		DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO.
		03/10/06	1" = 40'	82211	73160A	YUNG	21

FINAL R.O.W.			
AUTH	DATE	NO.	REVISION

TEST HOLE NO. 1

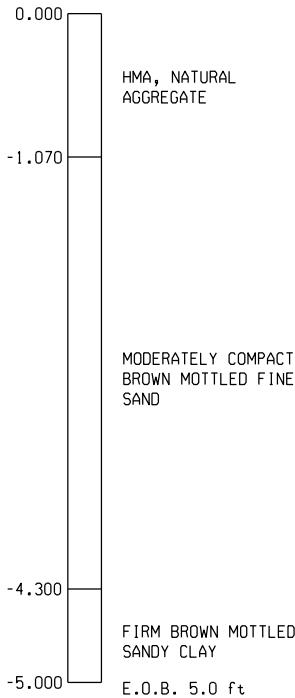
LOCATION STATION: 468+88.0 6.4 ft Rt. OF Lt. E.O.M.
SB M-85 (SLOW LANE)
WAYNE CO.
GROUND SURFACE ELEVATION: 0.000 ft BORING DATE 9/27/05



CORE TAKEN:
15.0' NORTH OF RAILROAD TRACKS
(UTILITIES RUNS 10.0' NORTH OF
RAILROAD TRACKS)
APPROXIMATE GPS COORDINATES (+/-
10.0')
4207.699 N
8312.076 W

TEST HOLE NO. 2

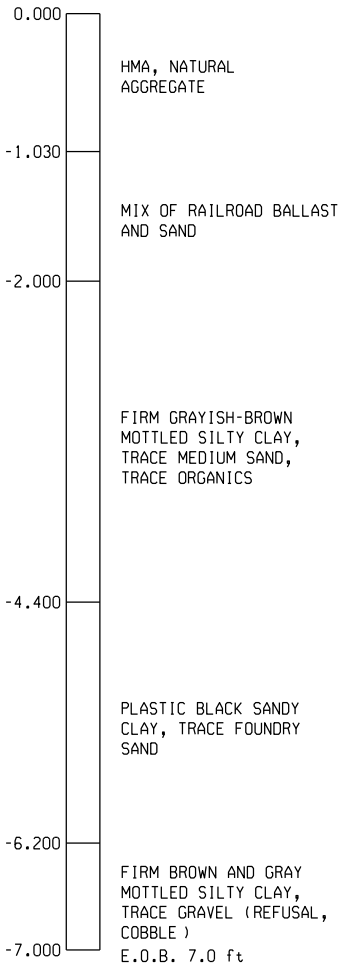
LOCATION STATION: 468+57.0 5.0 ft Lt. OF Rt. E.O.M.
NB M-85 (FAST LANE)
WAYNE CO.
GROUND SURFACE ELEVATION: 0.000 ft BORING DATE 9/27/05



CORE TAKEN:
50.0' SOUTH OF RAILROAD TRACKS
APPROXIMATE GPS COORDINATES (+/-
10.0')
4207.689 N
8312.080 W

TEST HOLE NO. 3

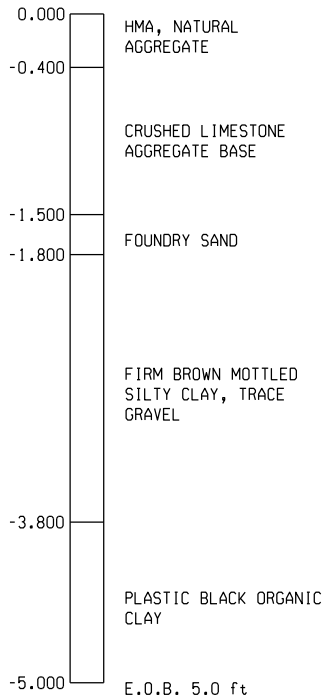
LOCATION STATION: 468+25.0 6.0 ft Lt. OF Rt. E.O.M.
NB M-85 (SLOW LANE)
WAYNE CO.
GROUND SURFACE ELEVATION: 0.000 ft BORING DATE 9/27/05



CORE TAKEN:
10.0' SOUTH OF RAILROAD TRACKS
APPROXIMATE GPS COORDINATES (+/-
10.0')
4207.692 N
8312.028 W

TEST HOLE NO. 4

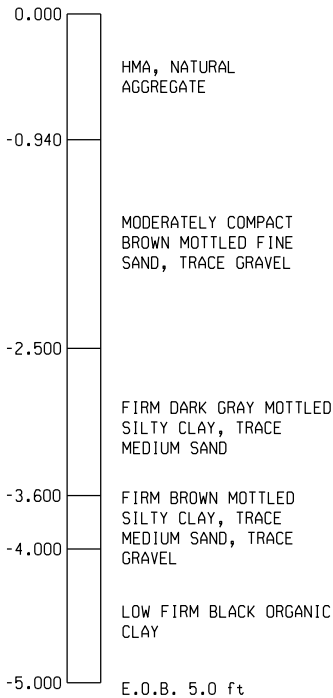
LOCATION STATION: 468+20.0 5.0 ft Rt. OF Rt. E.O.M.
NB M-85 (OUTSIDE SHOULDER)
WAYNE CO.
GROUND SURFACE ELEVATION: 0.000 ft BORING DATE 9/27/05



CORE TAKEN:
20.0' SOUTH OF RAILROAD TRACKS
APPROXIMATE GPS COORDINATES (+/-
10.0')
4207.690 N
8312.027 W

TEST HOLE NO. 5

LOCATION STATION: 467+65.0 5.0 ft Rt. OF Lt. E.O.M.
NB M-85 (FAST LANE)
WAYNE CO.
GROUND SURFACE ELEVATION: 0.000 ft BORING DATE 9/27/05



CORE TAKEN:
25.0' NORTH OF RAILROAD TRACKS
(UTILITY TRENCH RUNS 30.0' NORTH OF
RAILROAD TRACKS)
APPROXIMATE GPS COORDINATES (+/-
10.0')
4207.699 N
8312.026 W

NOTES:

0 1st 6 in
0 2nd 6 in
0 3rd 6 in

NUMBERS IN CIRCLES DENOTE NUMBER OF BLOWS REQUIRED TO DRIVE A 2" O.D. (1.5" I.D.) SPLIT SPOON SAMPLER 3 SUCCESSIVE 6" INCREMENTS USING A 140 lb HAMMER FALLING 30".

CONSISTENCY WAS DETERMINED BY INSPECTION OF SAMPLES AND SUBSTAINED BY SOILS RESISTANCE TO DRILLING TOOLS.

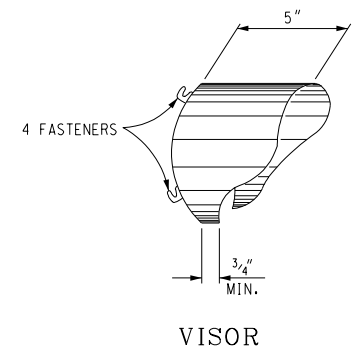
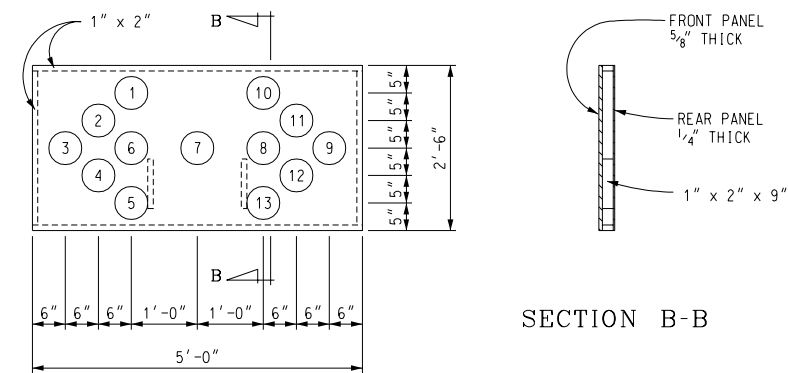
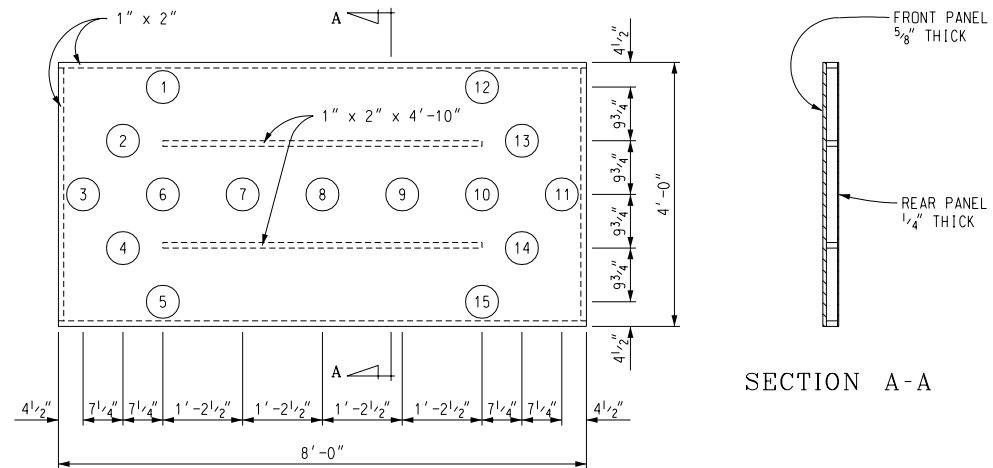
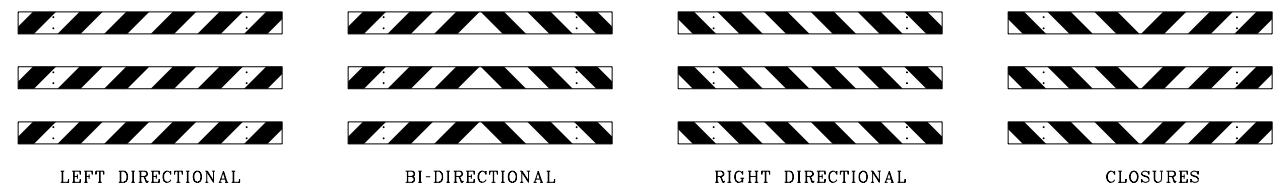
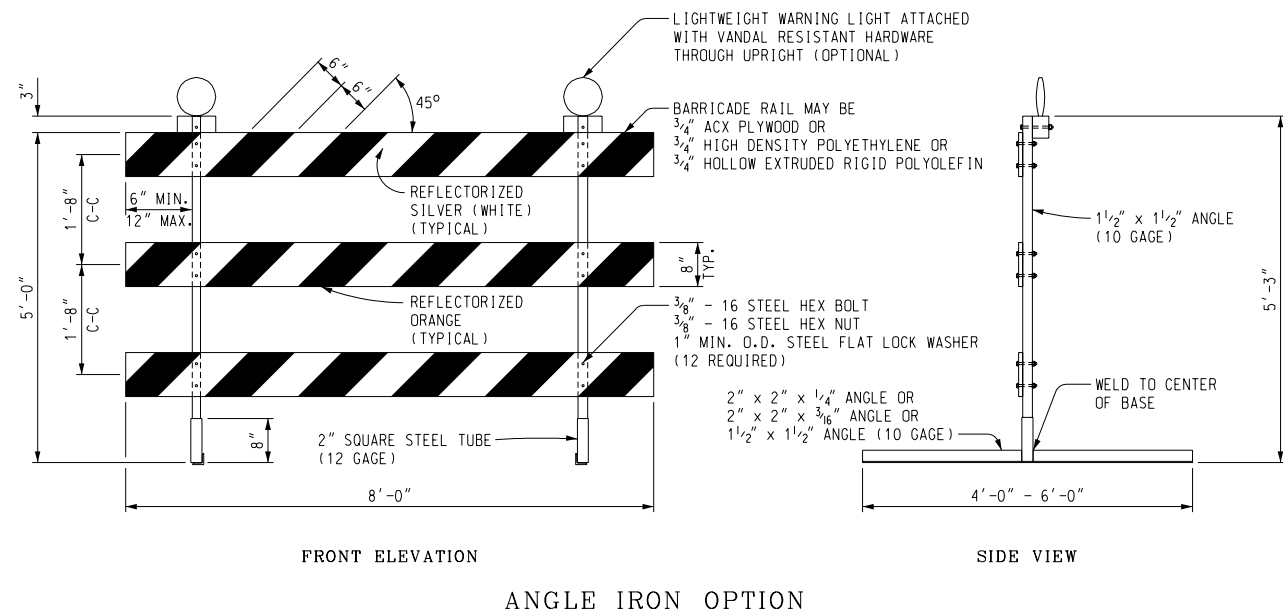
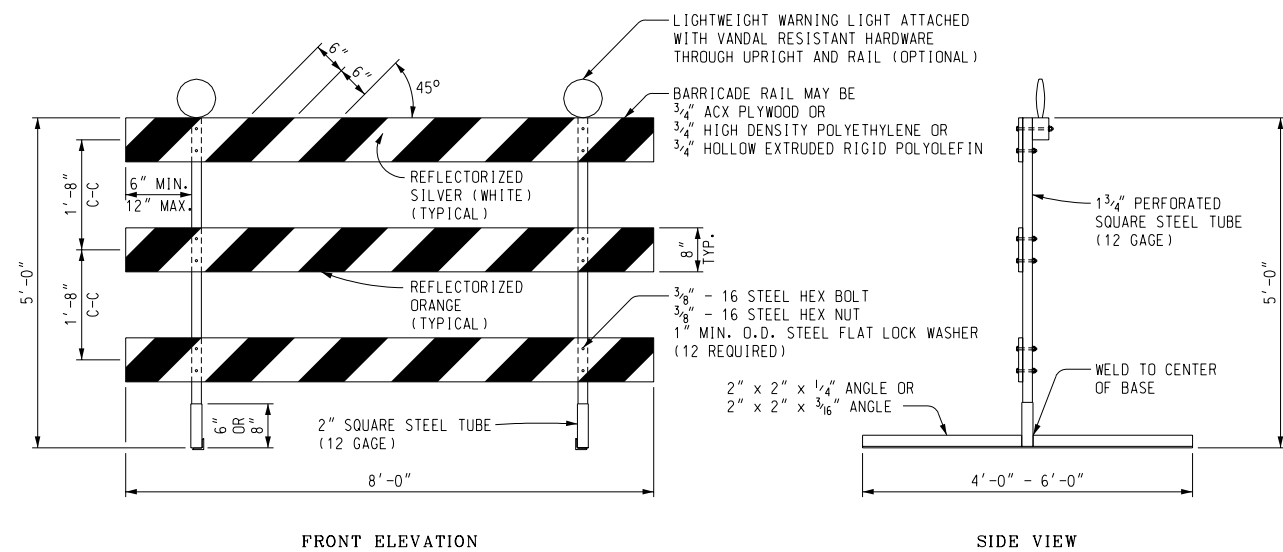
THE SOIL BORING LOGS REPRESENT POINT INFORMATION. PRESENTATION OF THIS INFORMATION IN NO WAY IMPLIES THAT SUBSURFACE CONDITIONS ARE THE SAME AT LOCATIONS OTHER THAN THE EXACT LOCATION OF THE BORING.

SOIL BORINGS



M-85 AT GO1 OF 82211, CN/GTW RAILROAD CROSSING

DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO.
03/10/06		82211	73160A	YUNG	R.O.W CONST.
					22

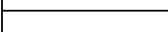


NOTE:

A STANDARD 6" DIAMETER CUTAWAY VISOR, SIMILAR TO THE ONE SHOWN, SHALL BE MOUNTED OVER EACH LIGHT. THE ARC ENCLOSED BY THE VISOR SHALL BE A MINIMUM OF 290 DEGREES. THE VISOR SHALL BE OF SHEET CONSTRUCTION AND OF NON-FERROUS, CORROSION-RESISTANT MATERIAL WITH A MINIMUM THICKNESS OF 0.045". IT SHALL BE PAINTED FLAT BLACK INSIDE AND OUT. THE AXIS OF THE VISOR SHALL BE PERPENDICULAR TO THE PANEL.

OPERATION MODES USE LIGHTS AS FOLLOWS				
LIGHTED ARROW	LEFT ARROW	RIGHT ARROW	LEFT-RIGHT ARROW	BAR
TYPE C	1, 2, 3, 4, 5, 7, 8, 9, 10, & 11 (10 BULBS)	3, 6, 7, 8, 9, 11, 12, 13, 14, & 15 (10 BULBS)	ALL EXCEPT 6 & 10 (13 BULBS)	** 3,7,8, 9, & 11 (5 BULBS)
TYPE B	1, 2, 3, 4, 5, 6, 7, 8, & 9 (9 BULBS)	3, 6, 7, 8, 9, 10, 11, 12, & 13 (9 BULBS)	ALL (13 BULBS)	3, 6, 7, 8, & 9 (5 BULBS)

** BULBS 6 & 10 ARE OPTIONAL IN THE BAR MODE OF THE LIGHTED ARROW-TYPE C.

 <p>MDOT Michigan Department of Transportation</p> <p>PREPARED BY DESIGN SUPPORT AREA</p> <p>DRAWN BY: <u>B.L.T.</u></p> <p>CHECKED BY: <u>W.K.P.</u></p>	<p>DEPARTMENT DIRECTOR Gloria J. Jeff</p>	<p>MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR</p>			
	<p>APPROVED BY: _____ ENGINEER OF DELIVERY</p>	<p>TEMPORARY TRAFFIC CONTROL DEVICES</p>			
	<p>APPROVED BY: _____ ENGINEER OF DEVELOPMENT</p>	<p>E.H.W.A. APPROVAL</p>	<p>11-17-2005 PLAN DATE</p>	<p>R-125-D</p>	<p>SHEET 1 OF 4</p>

MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR			
TEMPORARY TRAFFIC CONTROL DEVICES			
<u> </u> F.H.W.A. APPROVAL	<u>11-17-2005</u> PLAN DATE	R-125-D	SHEET 2 OF 4
CONTROL SECTION 82211		JOB NO. 73160A	SHEET NO. 23

SHEET NO.
24